

## ADVISORY DESIGN REVIEW COMMISSION

**Agenda** for the Advisory Design Review Commission Meeting of the Village of Pemberton to be held Tuesday, April 4th, 2017 at 5:00pm at 7400 Prospect Street.

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	Total Pages
<b>1. CALL TO ORDER</b>	
<b>2. MEMBERS &amp; STAFF INTRODUCTION</b>	
<b>Appointment of Chair &amp; Co-Chair</b>	6
• Bylaw 626	
<b>3. MINUTES</b>	
• Minutes of the ADRC Meeting of August 10, 2016	5
<b>4. DEVELOPMENT PERMIT APPLICATION (DPA008)-CRESTLINE</b>	
• Memo to ALUC	39
<b>5. NEW BUSINESS</b>	
<b>6. NEXT MEETING</b>	
<b>7. ADJOURNMENT</b>	

**THE VILLAGE OF PEMBERTON  
BYLAW NO. 626, 2009  
Advisory Planning Commissions Bylaw**

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**WHEREAS** pursuant to Section 898 (1) of the *Local Government Act* a Council may, by bylaw, establish an advisory planning commission to advise council on matters respecting land use, community planning or proposed bylaws and permits under Divisions 2, 7, 9 and 11 of the *Local Government Act*,

**AND WHEREAS** pursuant to Section 898 (3) of the *Local Government Act* a bylaw establishing an advisory planning commission must provide for the commission's composition, method for the appointment of members, the procedures governing the commission's conduct and referral of matters to be considered;

**THEREFORE BE IT RESOLVED** that the Municipal Council of the Village of Pemberton enacts an Advisory Planning Commission Bylaw as follows:

**1. This Bylaw may be cited as the "ADVISORY PLANNING COMMISSIONS BYLAW NO. 626, 2009".**

**2. The Village hereby establishes two distinct Advisory Planning Commissions known as: "Design Review APC" and "Land Use APC".**

**3. Composition**

(1) The "Design Review APC" shall be composed of six (6) members. One (1) member shall be a Director of the Pemberton and District Chamber of Commerce as selected by this organization.

(2) The background and qualifications of the remaining five (5) members of the "Design Review APC" should provide design and development expertise that is appropriate to the Pemberton context. Council should make best efforts to appoint the following professionals and/or community members with specific knowledge of:

- architectural design;
- community planning;
- construction/development; and
- landscape design.

(3) Two thirds (2/3<sup>rds</sup>) of the individuals appointed as members to the "Design Review APC" shall be qualified as follows:

- a resident of the Village; or
- a resident of Squamish Lillooet Regional District's Area C; or
- a property owner in the Village for a minimum of one year.

- (4) The “Land Use APC” shall be composed of six (6) members.
- (5) The background and qualifications of the members should be reflective of an array of interests and perspectives within the Pemberton community. Council should make best efforts to balance the make-up of the “Land Use APC” and appoint members that have specific knowledge in one or more of the following areas:
  - agriculture;
  - children and youth;
  - environment stewardship;
  - land development/ construction;
  - local education;
  - recreation;
  - seniors/accessibility; and
  - tourism.
- (6) All individuals appointed as members to the “Land Use APC” shall be qualified as follows:
  - a resident of the Village; or
  - a resident of Squamish Lillooet Regional District’s Area C; or
  - a property owner in the Village for a minimum of one year.

#### **4. Appointments**

- (1) Members of the “Design Review APC” and “Land Use APC” shall be appointed by council for a term of two years. Appointments will be staggered by one year and members will be appointed in conjunction with council’s committee appointments that occur annually in December. Notwithstanding, the inaugural Commission will appoint half of its members to a one year term to facilitate staggered appointments in the future.
- (2) Members of the “Design Review APC” and the “Land Use APC” should not be appointed to more than three (3) consecutive terms.
- (3) Council may assign a member of council to both the “Design Review APC” and the “Land Use APC”, however, the council representative is not considered a member of the Commissions and therefore shall not be eligible to be an official (voting) member, but may attend in a resource capacity between council and the Commissions.
- (4) All members of the “Design Review APC” and the “Land Use APC” shall serve without remuneration, but may be paid reasonable and

necessary expenses that arise directly out of the performance of their duties.

## 5. Procedures for Governing Conduct

(1) Members of the “Design Review APC” and “Land Use APC” shall abide by the following Code of Ethics:

- attend all meetings except for reasons beyond their control, whether or not they feel that they have any useful input into the subjects under discussion;
- accept that if they miss more than three (3) meetings in any twelve (12) month period, they may be asked to resign;
- make best efforts to become fully informed of the possible effects of decisions related to proposed developments, policy or guidelines amendments or other specific planning directions; and
- hold themselves free of any conflicts of interest.

Any member who fails to adhere to the Code of Ethics will be asked to resign from the respective Commission.

(2) Meetings of the “Design Review APC” and the “Land Use APC”:

- shall be held in a timely manner as needed to address issues that arise from time to time or as referred by the Village;
- may set a regular meeting date and time, if deemed necessary;
- shall be open to the public;
- shall provide notice posted on the Village Notice Board and website, at least twenty-four hours in advance of the meeting, indicating the day, hour and place of the meeting;
- shall have a Village appointed Recording Secretary to keep the minutes of all meetings. The minutes shall be legibly recorded, signed by the chair or member presiding, and open for public inspection in accordance with the *Local Government Act*.
- may make rules by majority resolution, as it sees fit to govern its conduct noting that in so doing the intent is that in general the rules of parliamentary debate shall apply: members will address the chair, motions will be made and seconded, the Chairperson will call the question and the vote will be recorded.



- (3) The Chairperson of the “Design Review APC” and the “Land Use APC” shall be elected by vote of the members at the first meeting of the year.

The Chairperson:

- shall preside, when present, at any meeting and generally shall fulfill all of the duties usually performed by the Chairperson; and
  - shall have the same right of voting as the other members of the “Design Review APC” or “Land Use APC” and in case of an equality of votes for and against the question, the question is resolved in the negative and the Chairperson shall so declare.
- (4) The “Design Review APC” and the “Land Use APC” may elect one (1) of its members to be Vice-Chairperson, who in the absence of the Chairperson shall preside at meetings of the Commissions. In case the Chairperson is absent from a meeting and no Vice-Chairperson has been elected, the members present, if a quorum is present, shall elect one (1) of their members to act as Chairperson of that meeting.
- (5) Four (4) voting members constitutes a quorum for each the “Design Review APC” or the “Land Use APC”. A quorum is required to undertake:
- the transaction of business, and the decision;
  - all acts whatsoever authorized or required to be done, except as otherwise provided for; and
  - all questions of adjournment and others that may come before the Commissions.
- (6) Applicants for a particular bylaw amendment or permit are entitled to be given notice, attend and be heard at the corresponding Commission meetings.

## **6. Matters for Referral**

- (1) The “Design Review APC” shall review and provide council with recommendations on matters respecting Village design related issues particular to the following:
- Development permit and development variance permit applications;
  - Reports and policy analysis, notably development permit applications and guidelines;

- Planning and design documents (i.e. Official Community Plan, Village Vision and Zoning Bylaw), rezoning applications and reports that specifically relate to the form and character of a building, outdoor spaces and landscaping; and
  - Any other matters referred by council.
- (2) The “Land Use APC” shall review and provide Council with recommendations on matters respecting land use, community planning, major policy issues and proposed land use bylaws, permits and other applicable regulations, particular to the following:
- Reports and policy analysis including community and neighbourhood planning, housing, and economic development;
  - Applications for amendment of the Official Community Plan and Zoning Bylaw;
  - Major development applications; and
  - Any other matters referred by council.

## **7. Reporting Procedure**

- (1) The “Design Review APC” shall:
- provide input at early stages in the design review process, the timing of which will be determined on a case by case basis by the Manager of Development Services.
  - provide recommendations and comments with respect to matters outlined in Paragraph 6 (1) of this Bylaw. Recommendations shall be in the form of formal resolutions and shall be specific in their direction. The recommendations will be used by Village staff and their consultants when discussing a project with an applicant, and may be used by council when making decisions on development applications.
- (2) The “Land Use APC” shall
- provide input at early stages with regard to community planning and land use matters, the timing of which will be determined on a case by case basis by the Manager of Development Services.
  - provide recommendations and comments to the Village with respect to the matters outlined in Paragraph 6(2) of this Bylaw. Recommendations shall be in the form of formal resolutions and shall be specific in their direction. The recommendations will be used by Village staff and their consultants when discussing a

project with an applicant, and may be used by council when making decisions on land use and community planning issues.

- (3) Council will inform the “Design Review APC” and the “Land Use APC” of its decisions, yet council is in no way bound to the advice or recommendations of the commissions. Decisions not conforming to the “Design Review APC” and/or “Land Use APC” recommendations may be further discussed by the commissions should it be deemed appropriate or necessary by council.

#### **8. Staff Resources and Support**

- (1) The Manager of Development Services or their designate shall serve as a resource person.
- (2) The Manager of Development Services shall appoint a Recording Secretary to document the minutes of the meetings.

#### **9. General**

- (1) If any section, subsection or clause of this bylaw is for any reason held to be invalid by the decision of a court of competent jurisdiction, such decision will not affect the validity of the remaining portions of this bylaw.
- (2) This bylaw may not be amended or repealed and substituted unless Council first gives notice in accordance with section 94 of the *Community Charter*.

**READ A FIRST TIME** this 20<sup>th</sup> day of January, 2009

**READ A SECOND TIME** this 20<sup>th</sup> day of January, 2009

**READ A THIRD TIME** this 20<sup>th</sup> day of January, 2009

**RECONSIDERED, FINALLY PASSED and ADOPTED** this 3<sup>rd</sup> day of February, 2009

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Mayor

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Administrator

**VILLAGE OF PEMBERTON  
ADVISORY DESIGN REVIEW COMMISSION MINUTES**

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**Minutes** for the Advisory Design Review Commission of the Village of Pemberton held August 10, 2016 at 5:00pm at 7400 Prospect Street.

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**ADRC MEMBER IN ATTENDANCE:**

Caroline McBain  
Nicole Brink  
Bob Adams  
Mark Mendonca

**ADRC MEMBER REGRETS:**

Danielle Ellaway  
Sarah Allen

**STAFF IN ATTENDANCE:**

Tim Harris –Manager of Operations & Development Services  
Lisa Pedrini-Village Planner

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**1) CALL TO ORDER**

At 5:35pm the meeting was called to order.

**2) MINUTES**

Moved/Seconded

**THAT** the minutes of the ADRC Meeting of May 17, 2016 be approved as circulated.

**CARRIED**

**3) DEVELOPMENT PERMIT: DPA007-CRONIN PROPERTY(Form and Character)  
7414 Pioneer Street**

**Application Representatives:**

Pete Cronin-Owner

The Village Planner gave an overview:

- The land is zoned C-1 (Downtown) & is designated as Development Permit Area No. 4 – Downtown Revitalization.
- Single Residential Dwelling use is not an outright permitted use in the C-1 Zone.
- The existing residential use is considered legal non-conforming.
- The intent of this application is to obtain a DP to develop a new building that would combine commercial & residential use with a business professional office on the main floor and a single residential suite above.
- It has been challenging to make the application meet the requirements of the C-1 zoning due to the residential character of Pioneer Street.
- The OCP guidelines for Downtown Revitalization do not mesh well with the character of this small tucked away street.

- Typically, a property within C-1 zoning seeking a DP for Form & Character contributes to off-site improvements (i.e. sidewalks, street lights etc.) but due to the home being located on a small cul de sac street which is more like a lane in size (since the width of the road does not meet MMCD standards for road width, improvement long term is unlikely).
- It seems fitting to have homebased business in the area.
- The owner wants to retain the character but at the same time run a business onsite.
- The application is to consider the Form & Character of the design and the landscaping of the proposed development.
- Three (3) out of the six (6) trees onsite would need to be removed to accommodate the development.
- The site would require five (5) parking spaces as shown on the Memo to the ADRC. As per the Village Zoning Bylaw, the parking requirement = 2 for single family dwelling/1.1 parking for business & professional office/1.25 for the residential suite. The owner would have to apply for a variance should he wish to request a reduction in the parking requirements. However, applying does not necessarily mean that the reduction would be granted. Parking variance are often requested but historically seldom granted.
  
- The Planner suggested the Commission walk over to Pioneer Street for a brief site visit. Members and staff left the meeting room for approximately 10 minutes.

Upon their return, the Commission members discussed the details of the application.

- The owner gave an overview of the design. The owner commented that the parking requirements seemed high and that he would prefer not removing some trees to provide additional parking but he would be prepared to meet the zoning parking requirement.
- The owner advised of his intent is to use the cedar lumber from the cut trees.

It was Moved/Seconded

**THAT** the ADRC recommends that the current design of the development not be accepted by the Village due to a number of design details missing, and recommends that the applicant resubmit a revised finalized design that labels and details:

- North Arrows on all Site Plans
- Elevations on all four sides (including window types, sizes and locations)
- Dimensions of parking spaces
- Larger Overhangs (staircase and pathway/weather protection)
- Snow dumping areas
- How the south end of the building will be serviced/maintained
- Pathway from the parking area/residential home to the professional office entry
- Plantings proposed as part of the Landscape Plan

**CARRIED**

**4) DEVELOPMENT PERMIT: DPminor#102 – Revised Landscaping Plan  
Mount Currie Mobile Home Park/7470 Hwy 99**

**Application Representatives:**

None present

The Village Planner gave an overview of the application:

- The original application was submitted to the Village in the spring of 2015 proposing changes to the landscaping on the highway frontage.
- The application was at the time presented to the ADRC as an electronic referral. It was not supported as presented as the landscaping proposal to remove the mature cedar trees (currently unmaintained) included the sparse addition of low laying landscaping.
- Village staff recommended that the applicant contact staff to work together on a more cohesive landscaping plan.
- BCT Fencing Ltd. is acting as the agent to submit a revised plan.
- It is believed that the intent is to provide more visibility to the C4 Service Commercial business area (Silvhorn Automotive).
- The revised plan proposes the removal of the overgrown cedar trees, and the addition of a split-rail fence with some lower lying plantings and Dogwood Trees.
- The Plan does not include irrigation (Irrigation is a requirement of the Development Permit Area No. 7 – Gateway Development).
- The Official Community Plan landscaping guidelines for the Gateway DP area was listed:
  - The Development should be landscaped as to provide an attractive entry from Highway 99 and other public roads, specifically:
    - Incorporate landscaped areas within parking lots to break up large paved areas. The landscaping can also be used in the winter for snow storage areas.
    - Provide a landscaped buffer between land uses
    - Limit the use of fencing when not visible to public streets. Chain link fencing is not recommended.
    - Provide irrigation for all landscaped yards and open areas (excluding areas undisturbed in their natural state).

The commission members discussed the current proposal:

- All agree that the existing overgrown cedar trees are not attractive. However, some of the trees further down could be improved with significant trimming and enhanced with additional plantings. It was noted that the height of many existing trees is not supported by BC Hydro.
- This current proposal proposes to significantly improve the existing landscaping onsite.
- However, the deciduous trees would not provide coverage all year long. The type of tree proposed (Dogwood) also requires high maintenance.
- There was some concern that improving the entrance brings a street view to what is mostly the parking area of the business.
- It may be more attractive to include mixed clusters of trees rather than individual trees evenly spaced.

- The Commission also felt that using evergreens in the landscaped screen as a buffer would be a better option
- The recommended height of evergreens would be a minimum height of 3 to 4 meters.
- The split fence was not unanimously favoured by the Commission. It was felt that the fencing did not adequately buffer the land uses.
- The types of planting proposed would require irrigation and a lot of maintenance to ensure the natural grass in the general vicinity does not overtake them. Whose responsibility is it and who currently takes care of it?
- A berm would enhance the proposed plantings in front of the fence to define them as flower beds.
- A small amount of fencing combined with flower beds at the right turn entrance would be a nice feature.
- The Commission reiterated that the purpose of the landscaped buffer is not about hiding the trailer park, but rather creating an attractive entrance that screens the commercial use and the large gravel parking area.
- Maintenance of landscaping is very important, so if irrigation is not proposed, alternatively, the Village might consider retaining a bond for a year's time to ensure the viability of the trees.

It was Moved/Seconded:

**THAT** the ADRC supports the property owner's intention to make improvements to the entrance;

**AND THAT** the ADRC recommends that the property owner choose a hardy evergreen in order to ensure continuity with the existing cedar hedging (which if regularly maintained is capable of being quite attractive) and to create a year-round landscaped buffer between the Highway and the C-4 Zone;

**AND THAT** the applicant considers using a small portion of split rail fencing and berms to create an attractive landscaped feature at the right hand corner of the entrance using the proposed plantings.

**AND THAT** in lieu of the installation of an irrigation system, the applicant provide a one year bond for the survival of the landscaped materials (plantings and trees) and that the Village include a condition in the DP that the landscaping has to be maintained.

**CARRIED**

## 5) NEW BUSINESS

It was requested that the Village Staff contact the Pemberton Hotel to follow up on the conditions of their Minor Development Permit approval as discussed at the May 2016 ADRC meeting, in regards to the windows/signage improvements. More specifically, the installation of shutters on the rest of the windows & removal of the older liquor store sign.

**6) NEXT MEETING**

TBA (as needed)

**7) ADJOURNMENT**

At 7:30 p.m. the meeting was adjourned.

This is a true and correct copy of a meeting of the Advisory Design Review Commission of the Village of Pemberton, held August 10, 2016.

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Chair

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**Date:** April 4, 2017  
**To:** Advisory Design Review Commission  
**From:** Lisa Pedrini, Village Planner  
**Subject:** Major Development Permit No. 008 – Crestline

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The Village of Pemberton has completed an initial review of the Development Permit application received by the Village of Pemberton from Lovick Scott Architects on behalf of BC1057219 (Pemberton Landing GP Ltd). The development is a residential complex consisting of five townhouse buildings using passive house technology. The subject properties are Lots 1,2 and 3 on Plan 12595, DL 203, LLD and are located at 1422, 1426 and 1430 Portage Road (see Map attached as Appendix A). The proposed development is immediately west of the Portage Station development and south of the Pemberton Public Library. The property is designated within the following Development Permit Areas as identified in the Village's OCP:

- DP Area No. 2 – Land Constraints (Floodplain); and,
- DP Area No. 6 – Multi-family and/or Commercial Development

The purpose of this memo is to assist the Advisory Design Review Commission in its consideration of the application.

## **BACKGROUND**

The site is a collection of three vacant, serviced lots. Lot 3 formerly contained a duplex that was removed in the fall of 2016. The site is adjacent to the Portage Station development to the east, vacant Crown Land containing a BC Hydro right-of-way to the west and a vacant private lot to the south.

The Village of Pemberton has a lease over the Crown parcel (Lot 13) for public parking purposes and, given the recent adoption of the Community Agricultural Parks Master Plan, is in the process of amending the lease to facilitate the future development of Lot 13 into a community garden. The vacant adjacent property to the south currently only has frontage along Highway 99; however, site access may be provided in the future from the west, as development occurs to the south of the elementary school site.

Project Information Sign has been placed on the site as per Schedule G of the Village's *Development Procedures Bylaw No. 725, 2013*.



**DEVELOPMENT PERMIT/DEVELOPMENT VARIANCE PERMIT**  
**DPA008/DVPA118**  
**Lots 1-3, DL 203, Plan 12595, LLD**  
**1422, 1426, 1430 Portage Road**

**Andrea Scott, Agent for Pemberton Landing GP Ltd., has applied for permission to develop this site for multi-family residential use.**

	C-3 Zone	Variances Requested		
		Required	Proposed	Variance
Land Use: 5-Plex/35 stacked townhomes				
Gross Floor Area: 3,807 sq.m				
Parking Spaces: 68 (underground)				
Architectural Character: Mountain Modern				
Sustainability Feature: Passive House				
Max. Height	10.5 m	11.3 m	+0.80 m	
Min. Front Setback	4.5 m	4.34 m	-0.16 m	
Min. Rear Setback	4.5 m	4.34 m	-0.16 m	
Min. Side Setback	3.0 m	2.76 m	-0.24 m	
Max. Lot Coverage	40%	43.8%	+3.8%	

**WANT MORE INFO?**



Phone  
(604) 894.6135



Email  
admin@pemberton.ca



Website  
www.pemberton.ca

[www.pemberton.ca](http://www.pemberton.ca)

Figure 1 - Development Notice

### BRIEF DESCRIPTION OF THE PROPOSAL

As illustrated in **Attachment A**, the proposal is to develop five, three (3) storey residential stacked townhouse buildings containing a total of thirty-five (35) units on a 2,864 square metre (30,830 square foot) site. There will be between five (5) and nine (9) residential units per building. In total, the development includes: eleven (11) one-bedroom townhouses; fourteen (14) two-bedroom townhouses; and, ten (10) three-bedroom townhouses.



Figure 2 - Conceptual Rendering

The development proposal includes sixty-eight (68) private parking stalls situated in an underground parking garage. The proposed access is along the Portage Road frontage, directly across from Cottonwood Street. Portage Road is designated as an arterial road by the Ministry of Transportation and Infrastructure (MOTI) and as a result there is a requirement for an access permit under Section 62 of the Transportation Act (Authorization of use or occupation of provincial public highways).

In tandem with this application the developer proposes to build an additional twenty-four (24) stall public surface parking lot on the adjacent Lot 13 to the west, pending BC Hydro approval in accordance with a high-level agreement with the Village. These spaces will be available to the public in general – visitors to the community garden and to the other amenities in the area. Provision of a public parking lot within 150 m of the development will allow the applicants to utilize the Village’s provision for payment in lieu of parking (Bylaw No. 522, 2004) for seven (7) spaces that are covenanted on Lot 3, DL 203, Plan 12595 (Portage Landing).

There will be private outdoor space for units located at grade level, and private rooftop patios for some upper floor units. Extensive landscaping and green space around the building will accentuate the at grade aspects of the lowest habitable floor to afford privacy.

The front entry to the residential development is facing Portage Road, with each unit gaining access off common outdoor sidewalks and breezeways between buildings.



Figure 3 – Front Elevation

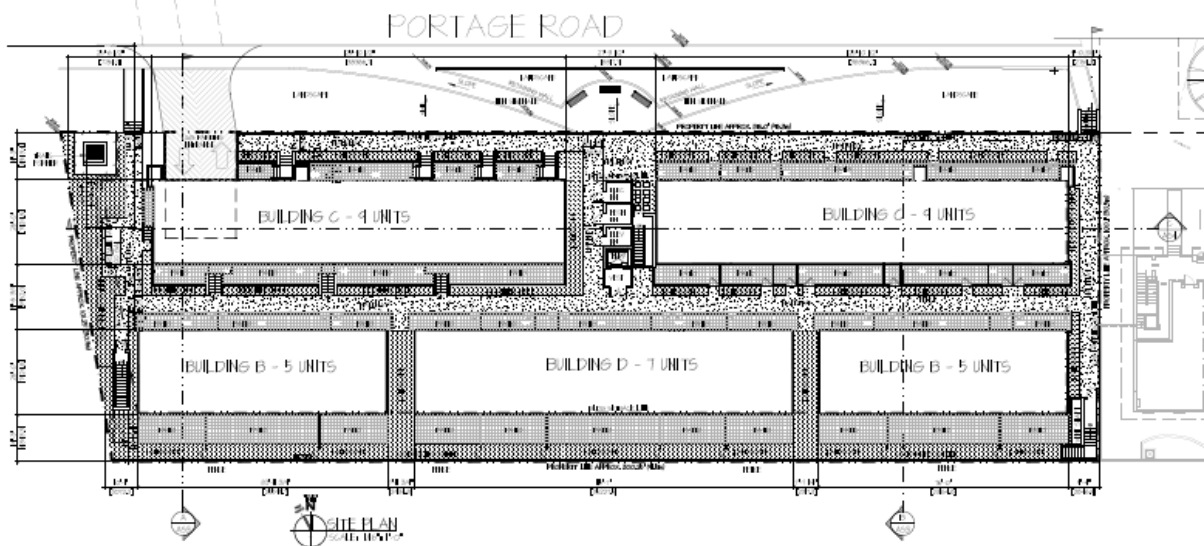


Figure 4 - Site Plan

The building has been designed to use passive house technology, facilitate healthy living and ease of a sustainable lifestyle. The majority of units will have private rooftop decks incorporated

into the roof structure. All units will also have large south facing windows that maximize the availability of natural lighting and heating. Most units will also have front patio spaces and private back yards. Ten (10) units in the complex will have private staircases that provide access to the second floor of two level units.

The building adheres to a quaint mountain rural theme and features a large gabled roof on the front buildings facing Portage Road. The building envelopes are simple rectangular shape, with articulated bump outs to add interest and privacy. The exterior finishes of the building include a combination of vertical siding, cedar shingles, rock columns and an exposed roof truss. The building will be built in a factory in Pemberton and assembled on site. The colour scheme would be deep red, slate and taupe as demonstrated on their Colour Board shown below as Figure 4.



Figure 4 - Exterior Finishes

No.	Material	Colour
1	Standing Seem Roofing	Charcoal Colour
2	Roof Hardie Trim	Painted – Benjamin Moore “Silver Springs”
3	Decorative Wood Braces & Columns	Natural – Maple Stain
4	Hardie Shakes	Painted light grey to match BM “Silver Springs”
5	Vertical Lap Siding	BM “Brick Red”
6	Hardie Window/Door Trim	BM “Silver Springs”
7	Wood Posts	Natural – Maple Stain
8	Stone Veneer Columns	Cultured Stone “Grey Cobblefield”



9	Hardie Panel Vertical Sidings – Stucco Texture	Colour to match BM “Kendall Charcoal”
10	Hardie Horizontal Trim	Painted – BM “Kendall Charcoal”

A landscaping plan has been provided and is attached as part of Appendix A.

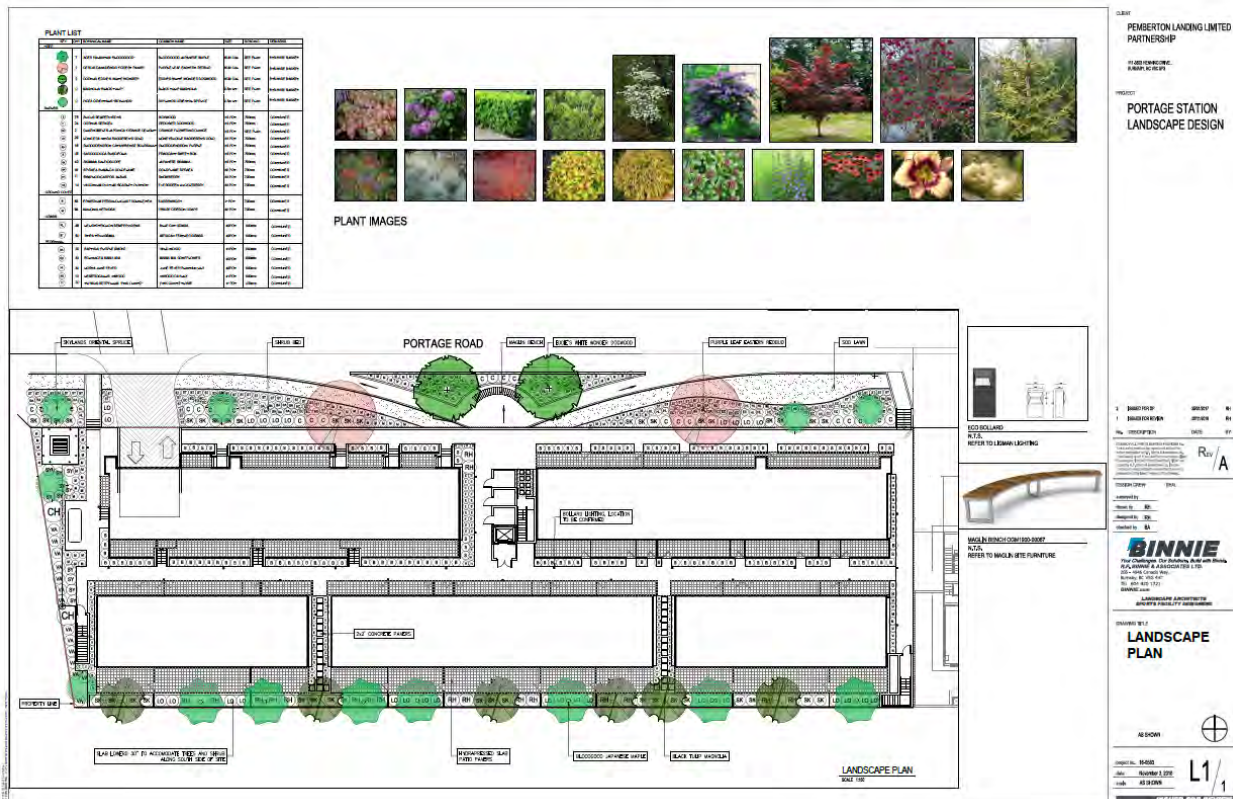


Figure 5 - Landscape Plan

### COMPLIANCE WITH ZONING BYLAW

The subject property is zoned Portage Road Commercial (C-3). The following is an assessment of the proposal in the context of the Zoning Bylaw requirements:

- a) Townhouses are a **permitted land use** in the C-3 zone, as per Zoning Amendment Bylaw No. 612, 2008. For townhouse uses in the C-3 Zone, there must be a minimum lot size of 900 m<sup>2</sup> and a minimum lot width of 25 m.
- b) The building would require a variance to comply with the 10.5-metre **maximum height** restriction based upon a calculation of average finished grade. The proposed maximum height is 37’1” (11.3.m).
- c) The building would require variances to comply with the front, rear, north interior and south interior **setback** requirements. Wider setbacks have been requested to compensate for wider walls that will be constructed to achieve passive house

standards for insulation. New front, rear and side setbacks have been proposed according to the following table:

<b>C-3 Zone</b>	<b>Required</b>	<b>Proposed</b>	<b>Variance</b>
Min. Front Setback	4.5m	4.34m	-0.16m
Min. Rear Setback	4.5m	4.34m	-0.16m
Min. Side Setback	3.0m	2.76m	-0.24m

- d) The building covers 43.8% of the lot and therefore would require a variance to comply with the 40% **maximum lot coverage** requirement. The Village has obtained a legal opinion from its solicitor to ensure that this variance would not constitute a variance to density (i.e. under the *Local Government Act*, a municipality may not vary use or density through a development variance permit).
  
- e) Based upon the requirement of 1.25 off-street parking spaces per each one (1) bedroom “townhouse” unit, 1.75 parking spaces per each two (2) bedroom “townhouse” unit, and 0.25 parking space per “townhouse” unit for visitors, sixty-eight (68) **residential parking spaces** would be required and are included in the underground parking garage.

## **COMPLIANCE WITH DEVELOPMENT PERMIT GUIDELINES**

The subject property is situated within Development Permit Area No. 6 – Multi-Family and/or Commercial Development and Development Permit Area No. 2 – Land Constraints. The guidelines for DP Area No. 6 address such issues as siting, building form, open spaces, circulation & parking and streetscape improvements & landscaping. The guidelines for DP Area No. 2 address slope, flood and wildfire concerns (in this case only the flood provisions are applicable). The following is an assessment of the proposal based on the DP Area No. 6 and DP Area No. 2 guidelines. The guidelines are provided as **Attachment B**.

The Design Review Committee shall review the proposal, and provide comments on compliance with the DPA No. 6 and DPA No. 2 guidelines.

### **I. DP Area No. 6– Multi-Family and/or Commercial Development**

General and Specific Guidelines

#### **a) Siting**

The proposed development is sited so that all buildings have south facing windows to take advantage of natural heating and lighting. The rear buildings are sited lower than the buildings on Portage Road which also help maximize the amount of sun received by all units in both front and rear buildings. Shared sidewalks and breezeways permeate the whole site and connect it internally and externally.

The site is also designed to minimize visual intrusion between neighbouring units within the development and outside the property. Each unit has a combination of private patios, decks and yards.

Related to the siting of buildings, two key considerations include: fire department access to the site; and, proximity of the front and rear buildings to each other (see Open Space, below).

#### **b) Building Form**

The buildings are all three stories and are designed with a quaint mountain rural aesthetic. The colours and building materials were chosen to mirror the theme and aesthetic portrayed by surrounding buildings to fit with the neighbourhood and reflect the Village's desire to have buildings that vary in visual appearance and have interesting street frontages.

The decision to build five smaller buildings rather than one large complex reflects the intention to keep the development at a small scale and to provide ample outdoor access for each unit. Units are also staggered to provide each unit with direct access to the outdoors.

It is noted that the building has underground parking and three above grade floors, with the first floor higher than street level; as a result, the apparent building height is slightly higher than three storeys. The Village defines height as the highest point on a flat roof or the mean height between the top of gable and the bottom of the roof eave. The top of the gable on the proposed building is 13.9 metres and the mean height is 11.3 metres, which requires a height variance since under current zoning, the maximum height is 10.5 metres.

The height of the development has triggered concerns from the Fire Department. The Village's current ladder truck is 55 feet, which is suitable for combatting fires in buildings about three storeys tall. With the building's proposed height of just over the typical three storey limit, there are concerns that the Fire Department's current equipment may have difficulties putting out a fire and/or extending to the upper/far reaches of the development.

#### **c) Construction Materials**

Building materials were chosen to complement the surrounding neighbourhood character and to withstand weather in all seasons. Building materials for exterior cladding include stone, cedar and glass. Additional detail may be required in relation to the materials/design details for the wall adjacent to the landscaping along the frontage of the development.

The roof lines on the front buildings complement neighbouring buildings.

#### **d) Streetscape Improvements and Landscaping**

The landscaping plan includes a variety of common ornamental and local plants that are incorporated into the overall site plan. Pemberton adopted a Landscape Plant List in 2011 that prioritizes local plants. Most of the proposed plants on the landscaping plan are not on the Pemberton Planting list. Many are similar; however, it is recommended that the developer review the Village of Pemberton Plant List that was adopted in 2011 to ensure that native species are prioritized.

The existing sidewalk along Portage Road has numerous driveway letdowns that will need to be upgraded or removed with development. Letdowns that are unnecessary should be removed. A few of the sidewalk panels are also damaged and will need to be replaced as the site is developed.

The landscaping that fronts Portage Road is within a Ministry of Transportation and Infrastructure (MOTI) road right of way. The building owner/building strata corporation would be responsible for maintenance. Plants should be hardy enough to survive damage from

snow clearing on Portage Road by MOTI and snow clearing on sidewalks by the Village of Pemberton.

**e) Circulation and Parking**

Access to the site aligns with Cottonwood Road and will require a Professional Traffic Engineer to conduct review of access management for the Ministry of Transportation and Highway's approval. In addition, the applicants will need to secure an Access Permit from the Ministry of Transportation and Infrastructure. Since Portage Road is designated an Arterial Highway under Section 62 of the *Transportation Act*, the applicants are advised that no parking is permitted on Portage Road, including temporary parking for loading or unloading. Consideration may be required for the provision of signage/lighting to assist in preventing pedestrian/vehicle conflicts at the vehicular access point to the site.

Most circulation around the site will be pedestrian and is accommodated by interconnected sidewalks and pathways. The main pedestrian access to the development is raised from the existing sidewalk level. A sloped access is proposed to reach the main on-site sidewalk.

Additional key circulation and parking considerations include solid waste collection and fire protection. Garbage and recycling facilities will be located in the parking garage underground and away from public view. Under the current site configuration, there is no loading bay. Clearance to the parking garage is 2.18 metres (7' 2"). Currently, solid waste collection vehicles would need to park on Portage Road, and bins will need to be brought to the street from the underground parking area.

From a fire safety point of view, the buildings are set higher than street level, making the apparent height of buildings taller than three storeys from street level, as discussed. The Fire Department has expressed concerns that the height of the buildings may be too high for their equipment and that the property does not have access to a lane. With the combination of building height and lack of rear lane access, the Fire Department may have difficulty reaching the rear buildings in case of emergency. The rear buildings are accessed via footpaths only.

**f) Snow management**

Vehicles access the parking garage directly from Portage Road. There will be a need to ensure that access to the parking garage is cleared from snow in the winter. Sidewalks and stairs that permeate and access the site will also need to be cleared to ensure access to all units. The combination of narrow spacing between buildings and high overall site coverage leaves little space to store snow that has been cleared from walkways. Further review may be required to identify snow clearing zones and determine where cleared snow will be stored on site during the winter months.

**g) Lighting**

The developer should provide a lighting plan for the site to illuminate all walkways through the site for safety. Adequate lighting will be especially important in between the buildings. Lighting should be down shielded to illuminate only desired areas and reduce light pollution and energy use.

**h) Crime Prevention Through Environmental Design**

The development has many sightlines through the property, and every unit has windows and doors that access and look on the shared spaces between buildings giving good coverage from an "eyes-on-the-street" perspective. However, the spaces between buildings are



relatively narrow, and there are spaces between the buildings that may not be visible from the street. Adequate lighting and surveillance will need to be ensured to discourage criminal behaviour.

**i) Open Spaces**

Each unit has its own decks, patios and/or yards. Patios/yards are provided on the ground floor, and rooftop patios are provided as well for some upper storey units. The developer has proposed reduced setback requirements to meet passive building requirements, which reduce the overall amount of open space at grade.

The buildings are set relatively close together, which reduces the amount of open space available on site. Appendix A provides some examples of similar townhouse developments in Vancouver and summarizes the distances between the buildings. As illustrated, in the proposed development, the distance between the two buildings is 6 metres (19.6 feet), whereas many comparable urban projects have a distance of about 8 metres (26.2 feet) to 11 metres (36.1 feet) between the front and rear buildings. An important difference in the Vancouver lots is that they are serviced by a laneway, and the typical lot depth is about 36.6 metres (120 feet) to 39.6 metres (130ft) compared to the 30.5 metre (100ft) lot depth on the Crestline property.

For the comparison Vancouver projects, there are typical floor area ratios of about 1.0 to 1.2. The proposed Crestline project has a current proposed floor area ratio of 1.33 based on a gross floor area of 3,807 square metres (40,978 square feet) and a site area of 2,864 square metres (30,830 square feet).

**II. DP Area No. 2 – Land Constraints**

The proposed site is not within the slope hazard or wildfire hazard zones that have been identified on Map L – Land Constraints in the Official Community Plan (OCP). The proposed site does fall within the floodplain of the Lillooet River and Pemberton Creek.

Professional reports and certification will be required that ensures that the development meets the flood construction level for the Lillooet River and Pemberton Creek and that the development will be able to compensate for debris from the Lillooet River and Pemberton Creek.

**COMPLIANCE WITH SIGN BYLAW**

A Comprehensive Sign Plan must be submitted to the Village for any proposed new building located in a Commercial Zone or development permit area. No sign shall be placed on a parcel that is not in conformance. Building Directories are permitted only as part of a Comprehensive Sign Plan. The applicant has not submitted its Comprehensive Building Sign Plan yet.

*Attachments:*

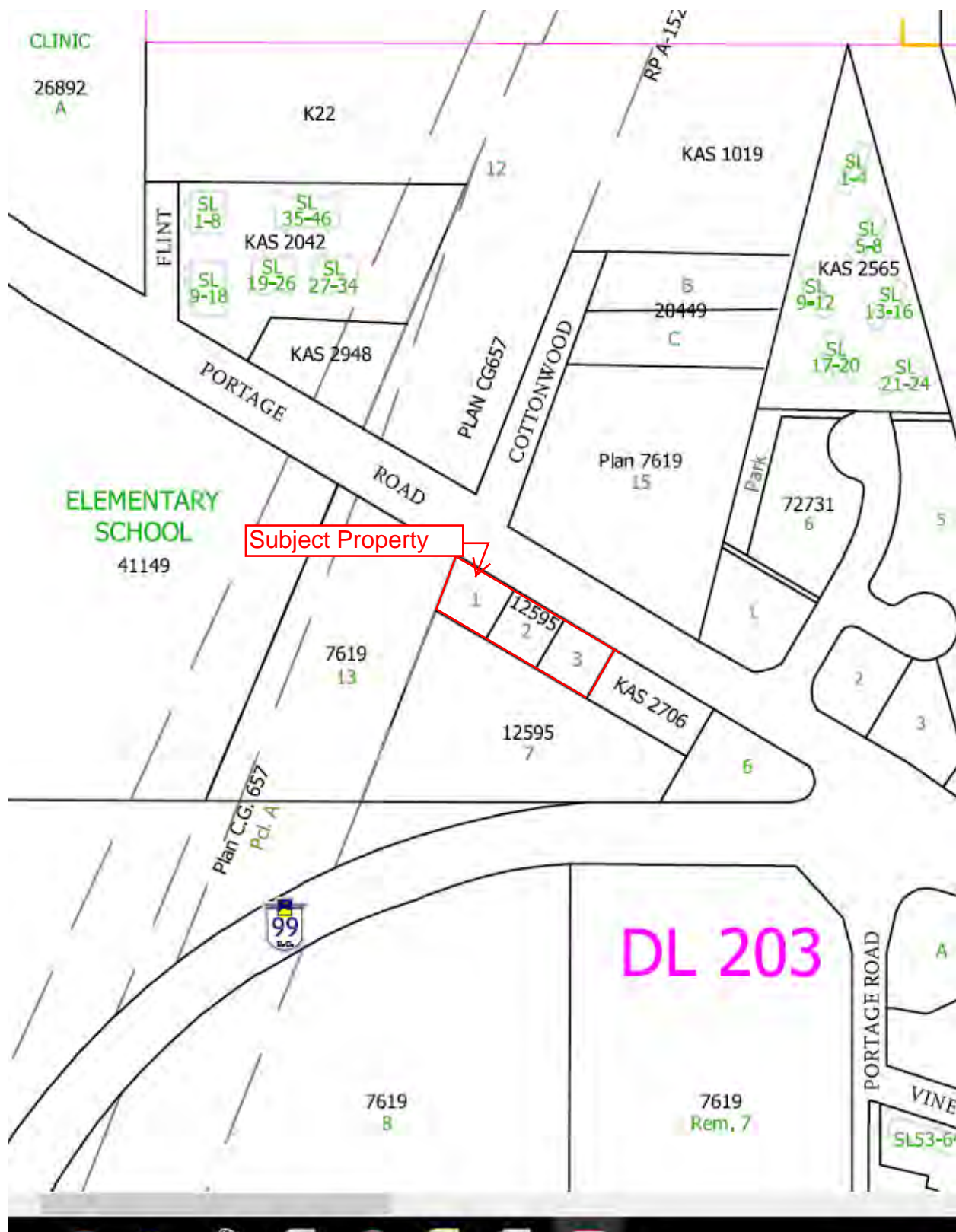
- A. *Subject Property Map*
- B. *Submission from Pemberton Landing GP Ltd.*
- C. *Development Permit Guidelines*

Memo to File - ADRC—Crestline  
April 4, 2017  
Page 10 of 10

Lisa Pedrini, Village Planner

cc. Tim Harris, Manager of Operations and Development Services

Location Map





**Crestline  
Lot 1, 2, 3 Portage Rd, Pemberton, BC**

We are proposing a new development on Portage Rd immediately west of the Portage Station development. The site will include 5 buildings with 3 storeys of residential townhomes and 1 level of underground parking.

The site is made up of 3 lots totaling 30,830 Sq. ft. (2,864 Sq.m.) of land. There are 35 residences proposed in this development with 68 parking stalls provided underground, as well as, 26 open storage units, garbage and recycling for the residences. There is a 2 storey elevator provided from the parking to the site above.

There are 3 building types containing either; 1 storey, 2 storey or 3 storey, one, two or three bedroom units. The majority of the units have private roof top decks, that are incorporated into the roof structure. All units have large south west facing backyards with decks and fences for privacy, as well as, front porches to provide an identity for each unit's front door. There are 10 units with doors on the ground floor that enter directly into a private set of stairs to access their 2<sup>nd</sup> floor, two level units.

We are requesting 3 variances for this project;

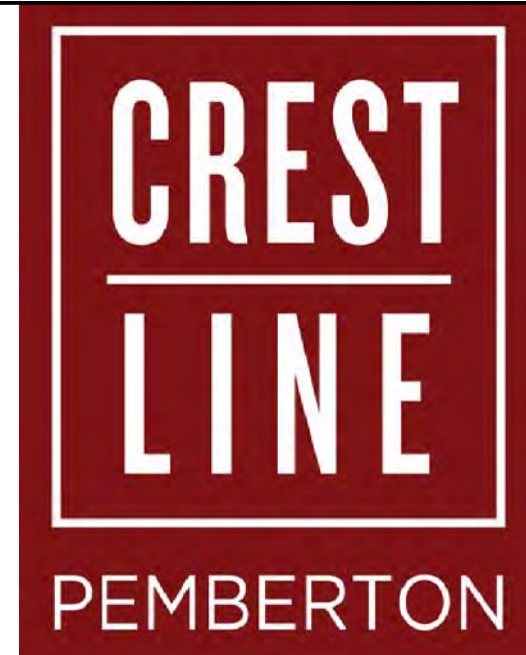
- To increase the height of the building from 10.5M to 11.3M
- To increase the site coverage from 40% to 43.8%
- To decrease the setback of the building at North, East & South property lines for the thickness of the Passive House panels – 0.16m, 0.16m & 0.24m

In keeping with the Quaint Mountain Rural theme, we have proposed a large gable feature on each of the buildings facing Portage Rd. With rock columns as the base, a combination of vertical siding and cedar shingles are topped off with an exposed roof truss under the sweeping gable roof. This project is intended to be built with Passive House standards and will be produced in the factory in Pemberton.









# Residential Development

1422, 1426, 1430 Portage Road, Pemberton, B.C.



## DRAWING LIST:

### ARCHITECTURAL:

- A0.00 COVER SHEET
- SURVEY
- CONTEXT PLAN
- CONTEXT PHOTOS
- RENDERING
- AS1 SITE / UNDERGROUND PARKING PLAN
- AS2 STREETScape
- AS3 SITE SECTIONS
- AS4 SITE SECTIONS
- A01 BUILDING B
- GROUND & SECOND FLOOR PLANS
- A02 BUILDING B
- THIRD FLOOR & ROOF DECK PLANS
- A03 BUILDING B
- ELEVATIONS
- A04 BUILDING C
- GROUND & SECOND FLOOR PLANS
- A05 BUILDING C
- THIRD FLOOR & ROOF DECK PLANS
- A06 BUILDING C
- ELEVATIONS
- A07 BUILDING D
- GROUND & SECOND FLOOR PLANS
- A08 BUILDING D
- THIRD FLOOR & ROOF DECK PLANS
- A09 BUILDING D
- ELEVATIONS

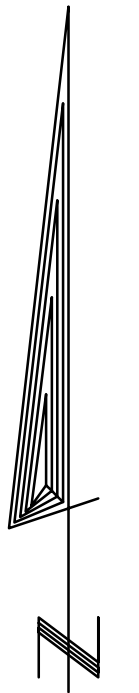
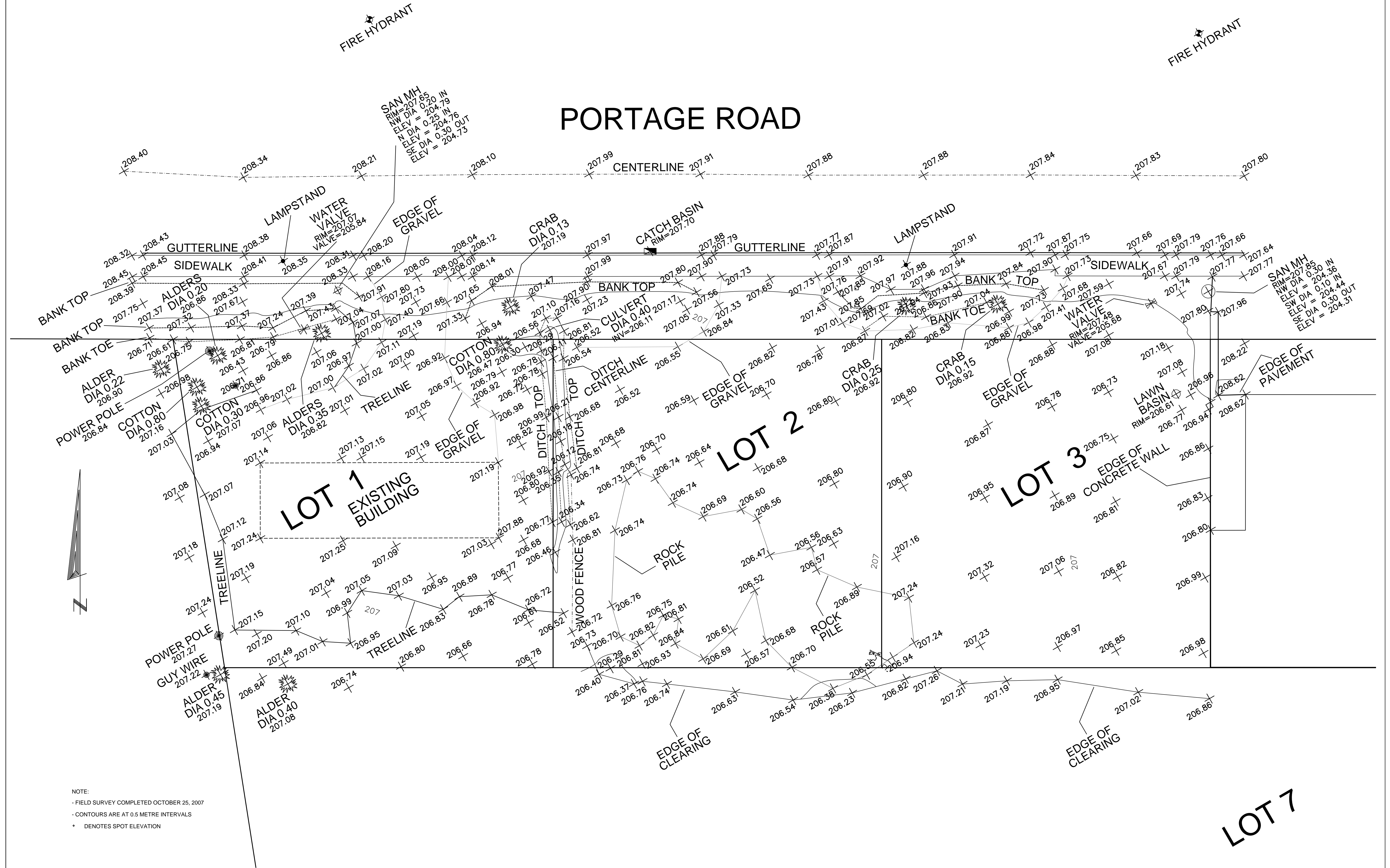
### LANDSCAPE:

- L1 of 1 LANDSCAPE PLAN

### SURVEY:

- SURVEY PLAN
- FILE : 03001CRD
- 07212TN1

Issued for DP - Jan 31st, 2017



NOTE:  
 - FIELD SURVEY COMPLETED OCTOBER 25, 2007  
 - CONTOURS ARE AT 0.5 METRE INTERVALS  
 + DENOTES SPOT ELEVATION

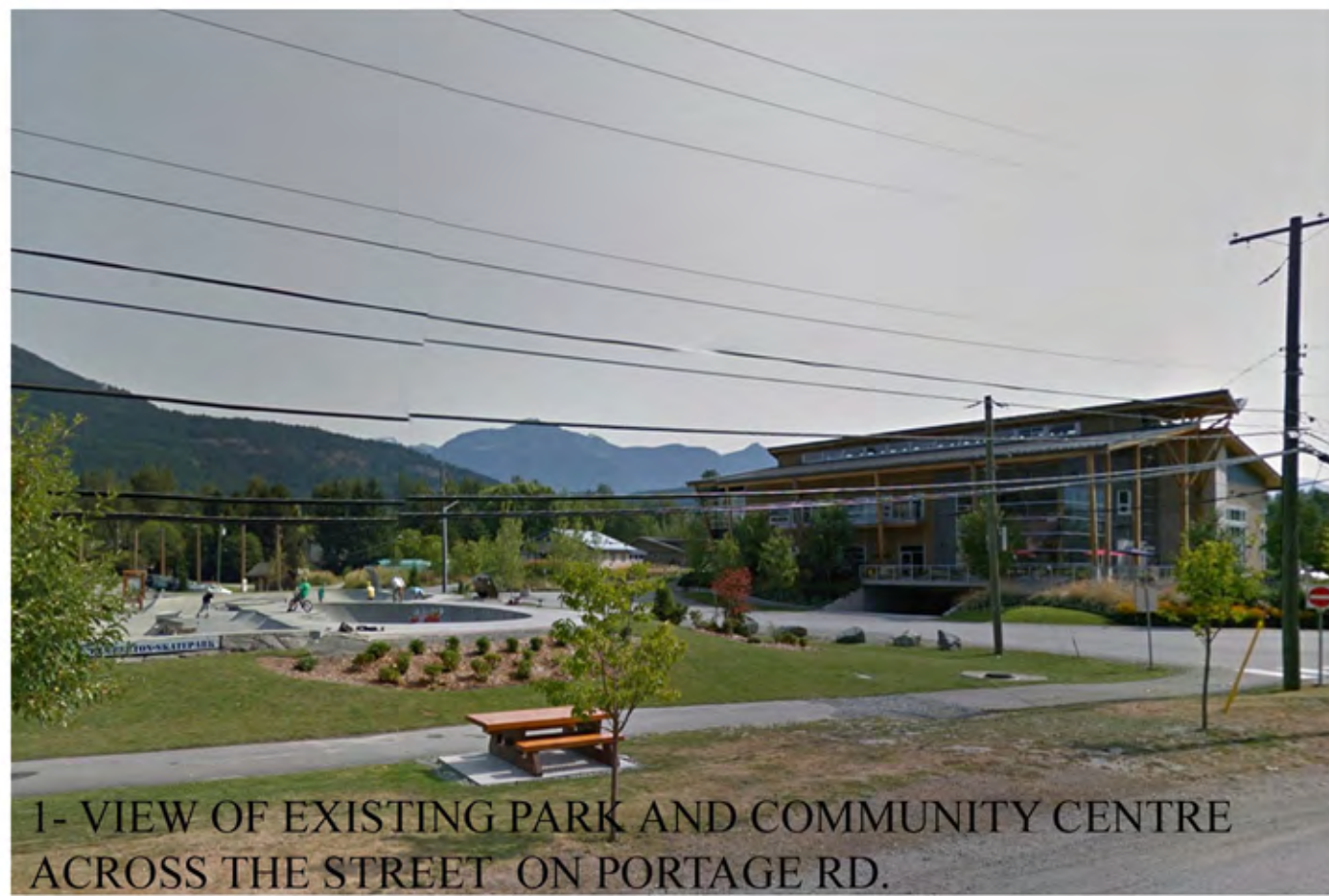
DOUG BUSH SURVEY SERVICES Ltd. Douglas J. Bush, ASCT, RSIS Applied Science Technologist #18 - 1370 Alpha Lake Road, Whistler, B.C. V0N 1B1 Phone 932-3314 Fax: 932-3039 dougb@dbss.ca	Revisions:	Notes: ELEVATIONS ARE GEODETIC DERIVED FROM W.R.S. MONUMENT NO. 851 LOCATED NORTH SIDE OF HIGHWAY 99 WEST OF HARROW ROAD ELEVATION USED = 207.178 METRES	Date: October 25, 2007 Files: 03001CRD 07212TN1	Client: WINDRIDGE PROPERTIES LTD. Project: LOTS 1-3 PORTAGE ROAD	SCALE: GRAPHIC SCALE METRES 1 : 250 All distances are in metres Job No. J07212 Drawing No. 07161D
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LOT 7









1- VIEW OF EXISTING PARK AND COMMUNITY CENTRE ACROSS THE STREET ON PORTAGE RD.



2- VIEW OF EXISTING ELEMENTARY SCHOOL ON PORTAGE RD.



3- VIEW OF EXISTING PROPERTY ACROSS THE STREET ON PORTAGE RD.



4- VIEW OF ADJACENT PROPERTY ON PORTAGE RD.



5- VIEW OF EXISTING GAS STATION ON SEA TO SKY HWY



6- VIEW OF VISITOR CENTRE ON SEA TO SKY HWY

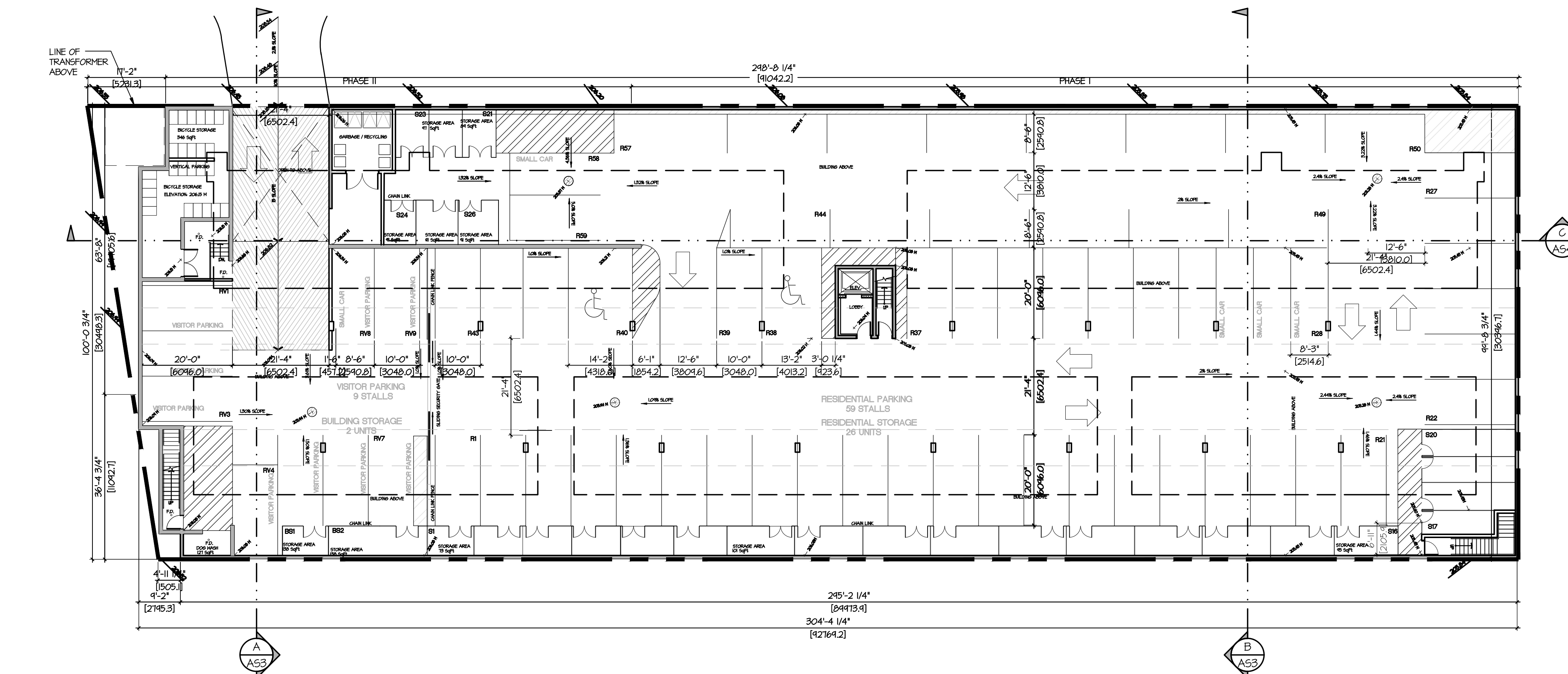
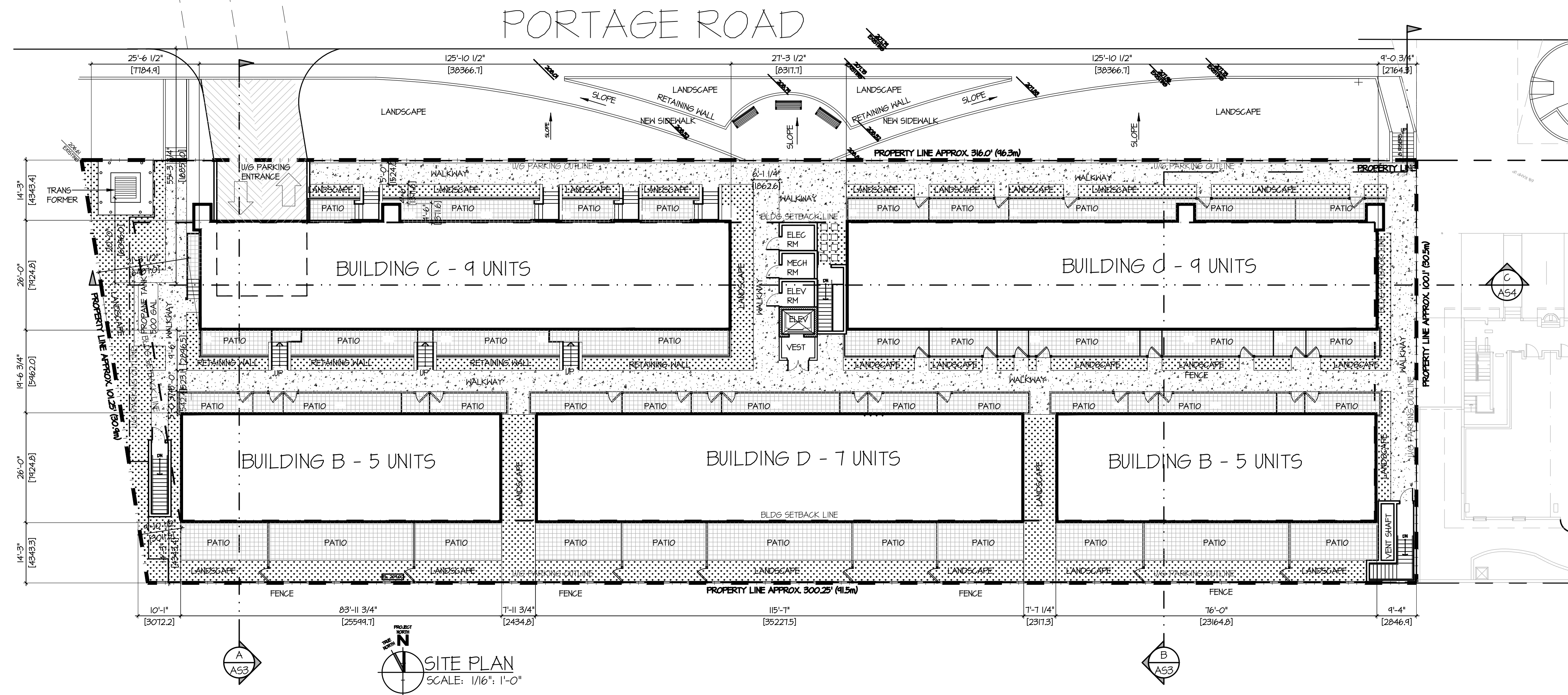


7- VIEW OF EXISTING HOTEL ON ARBUTUS ST.



8- VIEW OF PROPOSED SITE ON PORTAGE RD.





**SITE STATISTICS:**

LEGAL DESCRIPTION: LOTS 1, 2 & 3, DP 12545, DL 203  
 ADDRESS: PORTAGE RD, PEMBERTON, B.C.  
 ZONING: EXISTING: C3 - PORTAGE ROAD COMMERCIAL  
 SITE AREA: 30,830 Sq.Ft. (2,864 SM)  
 BUILDING AREA (GROUND FLOOR):  
 BUILDING C: 3,273 Sq.Ft. (304.1 Sq. M)  
 BUILDING C (MIRROR): 3,273 Sq.Ft. (304.1 Sq. M)  
 BUILDING B: 1,976 Sq.Ft. (183.6 Sq. M)  
 BUILDING D: 3,005 Sq.Ft. (279.2 Sq. M)  
 BUILDING B: 1,976 Sq.Ft. (183.6 Sq. M)  
 TOTAL BUILDING: 13,503 Sq.Ft. (1,254.5 Sq. M)  
 TOTAL UNITS: 35

SITE COVERAGE  
 ALLOWABLE: 40%  
 PROPOSED: 13,503 Sq. Ft. X 100 = 43.8% \*  
 30,830 Sq. Ft.  
 \* VARIANCE REQUIRED DUE TO PASSIVE HOUSING WALL THICKNESS

BUILDING HEIGHT  
 ALLOWABLE: 10.5m (34.5)  
 PROPOSED: 11.3m (37.0) \*  
 \* VARIANCE REQUIRED

SETBACKS REQUIRED PROVIDED  
 FRONT (N) 14'-9" (4.5m) 14'-3" (4.34m) \*  
 REAR (S) 14'-9" (4.5m) 14'-3" (4.34m) \*  
 SIDE (E) 9'-10" (3.0m) 9'-0 3/4" (2.76m) \*  
 SIDE (W) 9'-10" (3.0m) 9'-10 1/2" (3.01m)  
 \* VARIANCE REQUIRED DUE TO PASSIVE HOUSING WALL THICKNESS  
 WALLS EXTEND OVER SETBACK BY 6" & 9 1/4"

PARKING  
 CAR REQUIRED: TOWNHOUSE:  
 1 BEDROOM = 1.25 STALLS PER UNIT  
 2 BEDROOM = 1.75 STALLS PER UNIT  
 3 BEDROOM = 2 STALLS PER UNIT  
 11 x 1.25 = 13.75  
 14 x 1.75 = 24.5  
 10 x 2.0 = 20  
 TOTAL = 59  
 VISITOR STALLS:  
 0.25 STALLS PER UNIT  
 35 UNITS x 0.25 = 9  
 TOTAL PARKING STALLS REQUIRED: 68

PROPOSED:  
 UNDERGROUND PI  
 RESIDENTIAL STALLS  
 54 RESIDENTIAL STALLS (INCLUDING 2 HCAPP & 3 SMALL CAR)  
 9 VISITOR STALLS (INCLUDING 1 SMALL CAR)  
 TOTAL PARKING STALLS: 68  
 TOTAL STORAGE UNITS: 26

	Building B x 2 BLDGS	Building C x 2 BLDGS	Building D x 1 BLDG	Total
1 Bed & Den 2 storeys	1	4	1	11
2 Bed Unit Rancher	1	2	1	7
2 Bed Unit 2 storeys	1	0	1	3
2 Bed Unit & Den 3 storeys	0	0	4	4
3 Bed Unit 3 storeys	2	3	0	10
<b>Total</b>	<b>5</b>	<b>9</b>	<b>7</b>	<b>35</b>



REV	DATE	DESCRIPTION
01	01/31/17	ISSUED FOR DP

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DRAWN BY AS APPROVED P.J.L.

PROJECT CRESTLINE

1422, 1426, 1430 PORTAGE ROAD  
 PEMBERTON, B.C.

DRAWING

SITE PLAN  
 UNDERGROUND PARKING PLAN

PROJECT NUMBER DRAWING NUMBER

14-89 ASI

SCALE 1/16" = 1'-0"

DATE OCT 2014 REVISION JAN 31 17





STREETSCAPE ELEVATION  
SCALE: 1/16" = 1'-0"

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SCOTT**  
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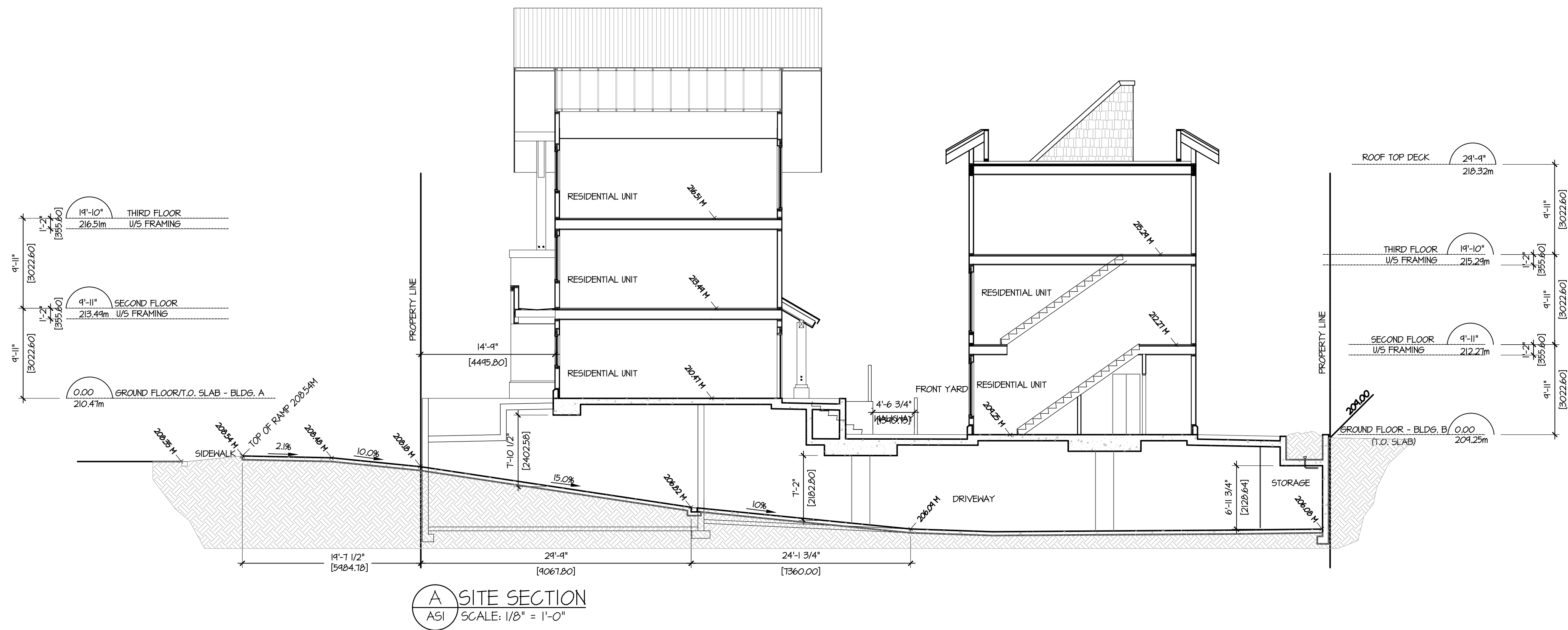
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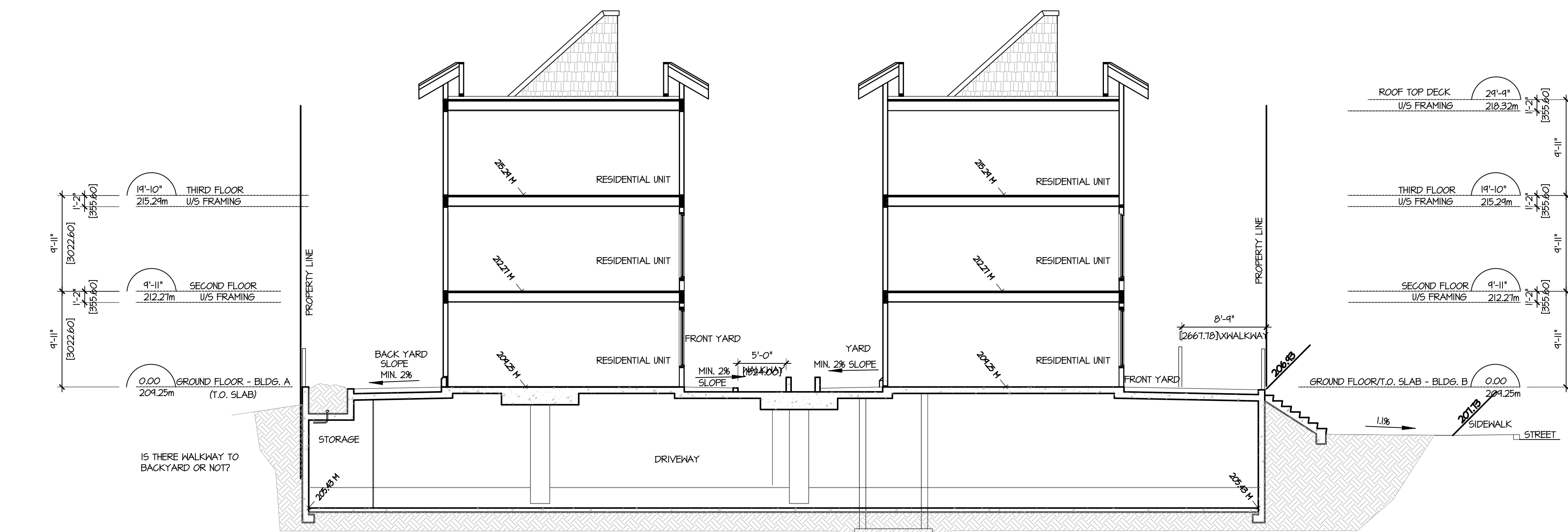
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**CRESTLINE**  
 1422, 1426, 1430 PORTAGE ROAD  
 PEMBERTON, B.C.

DRAWING  
 STREETSCAPE

PROJECT NUMBER	DRAWING NUMBER
14-89	AS2
SCALE	
1/16" = 1'-0"	
DATE	REVISION
OCT 2014	JAN 21 / 17



**A SITE SECTION**  
ASI SCALE: 1/8" = 1'-0"



**B SITE SECTION**  
ASI SCALE: 1/8" = 1'-0"

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PROJECT: CRESTLINE  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

DRAWING

SITE SECTIONS

PROJECT NUMBER: 14-89 DRAWING NUMBER: A53  
SCALE: 1/16" = 1'-0"  
DATE: 2017 REVISION: JAN 27TH 17



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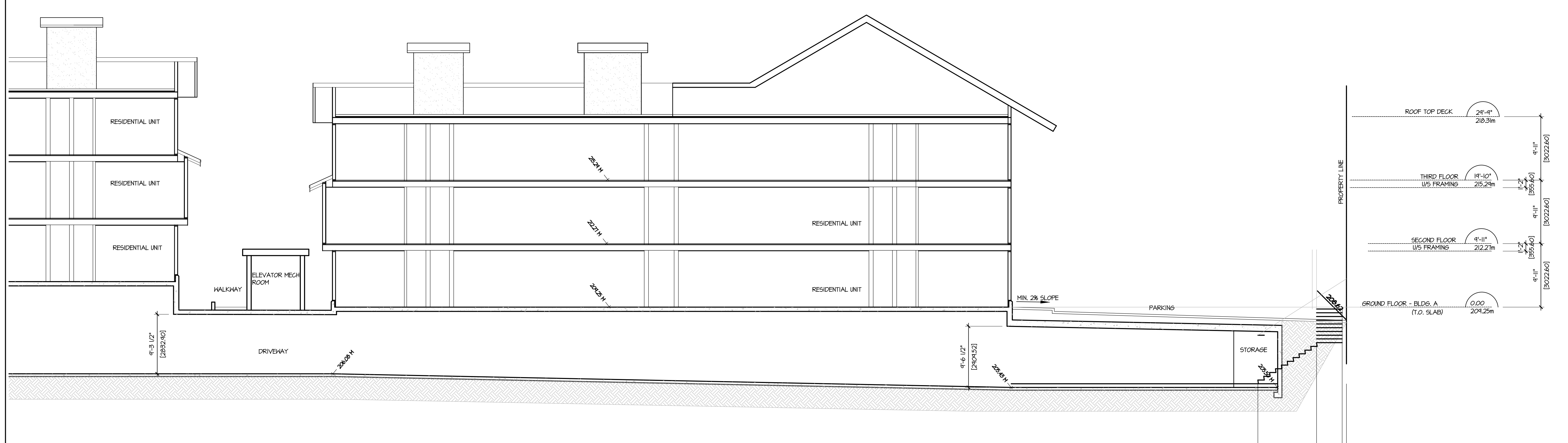
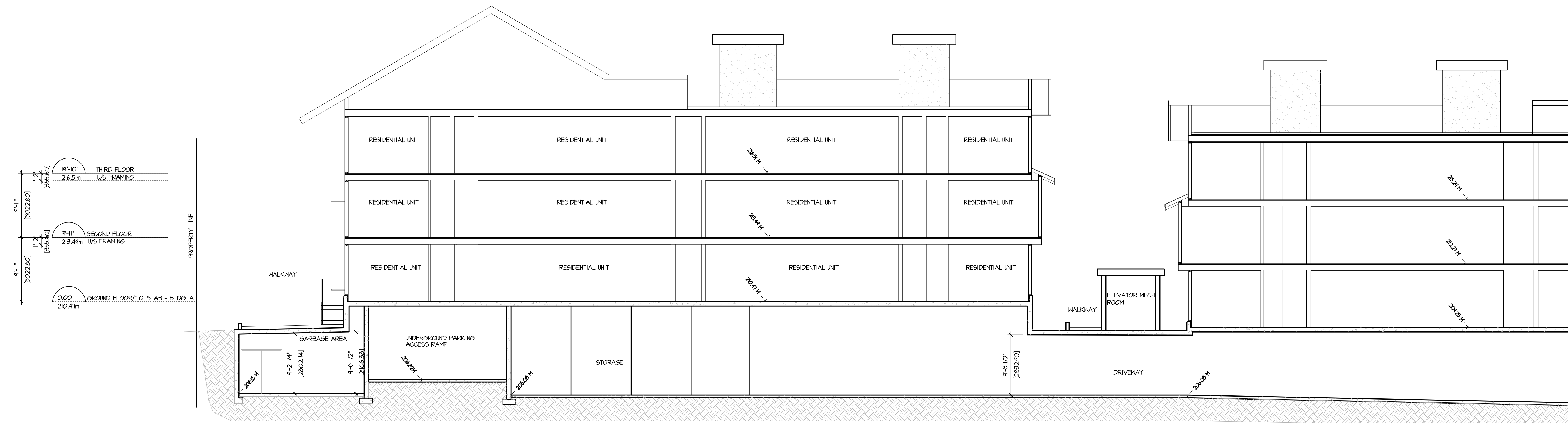
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1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

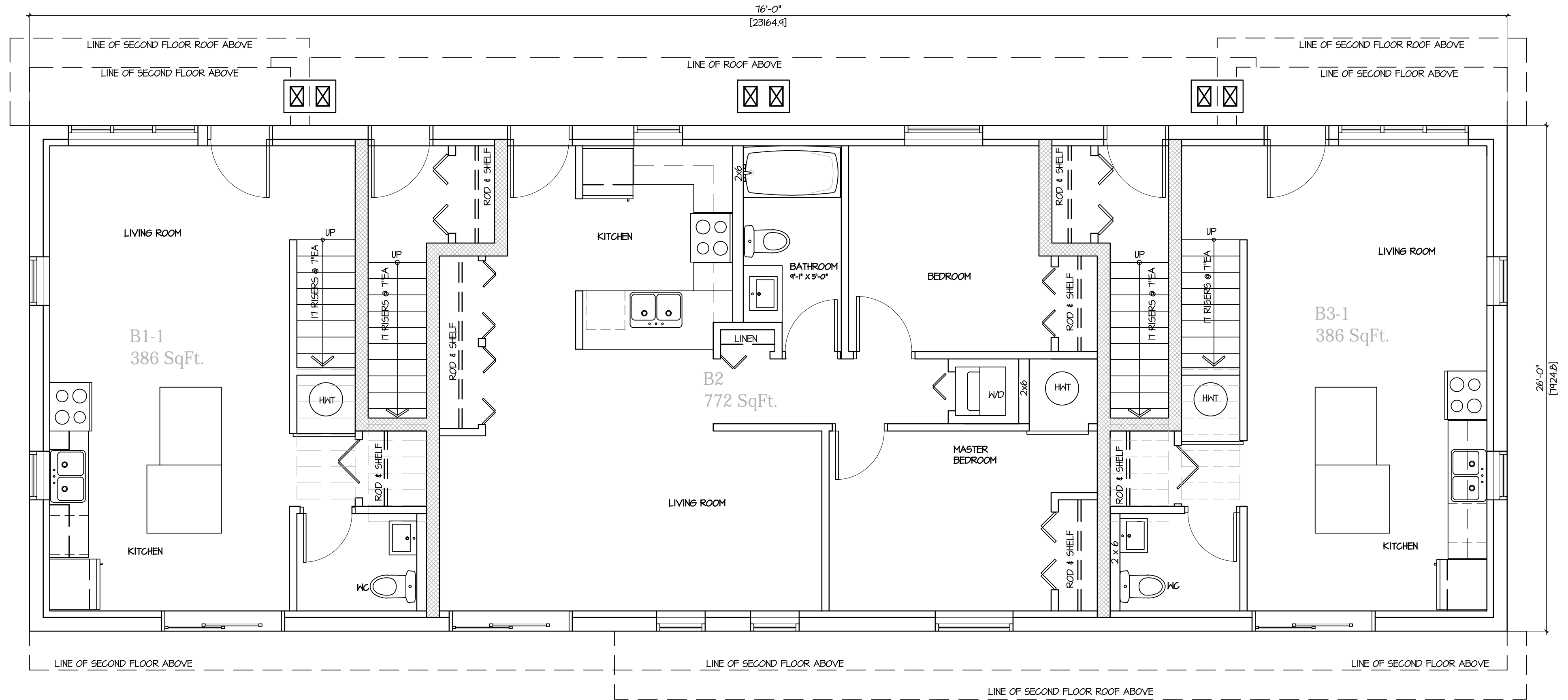
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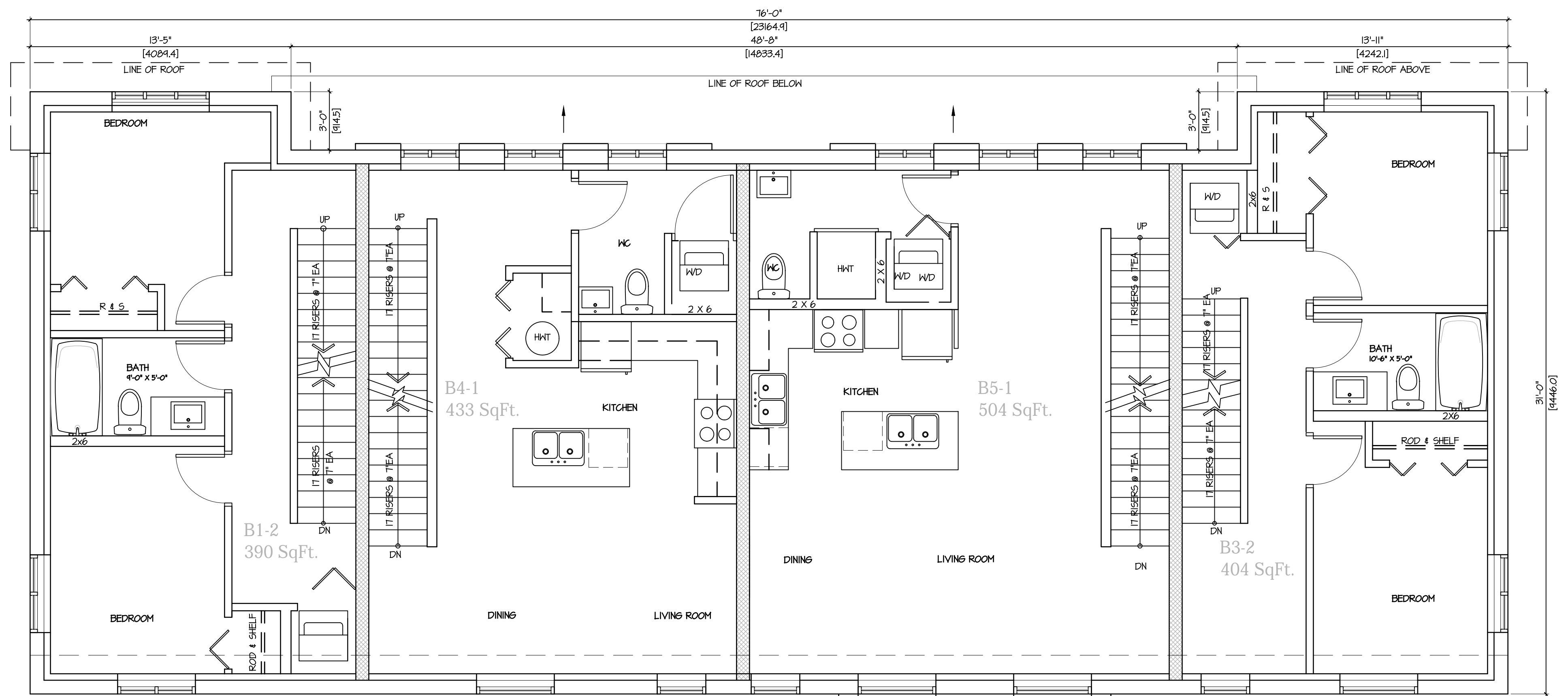
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14-89	AS4
SCALE	1/16" = 1'-0"
DATE	2017
REVISION	A

CRESTLINE 14-89





**BUILDING B - GROUND FLOOR**  
SCALE: 1/4" = 1'-0"



**BUILDING B - SECOND FLOOR**  
SCALE: 1/4" = 1'-0"

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PROJECT: CRESTLINE  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

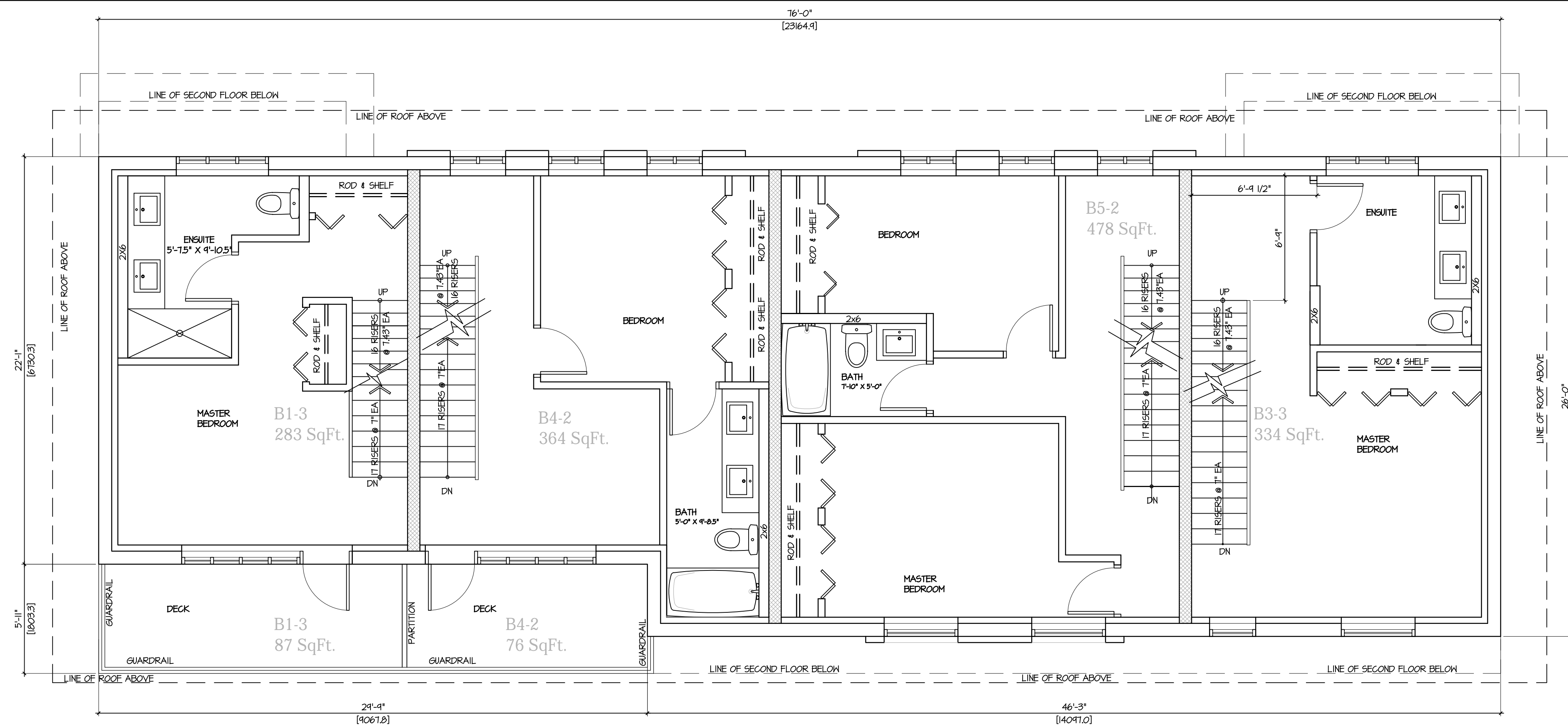
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GROUND & SECOND FLOORS

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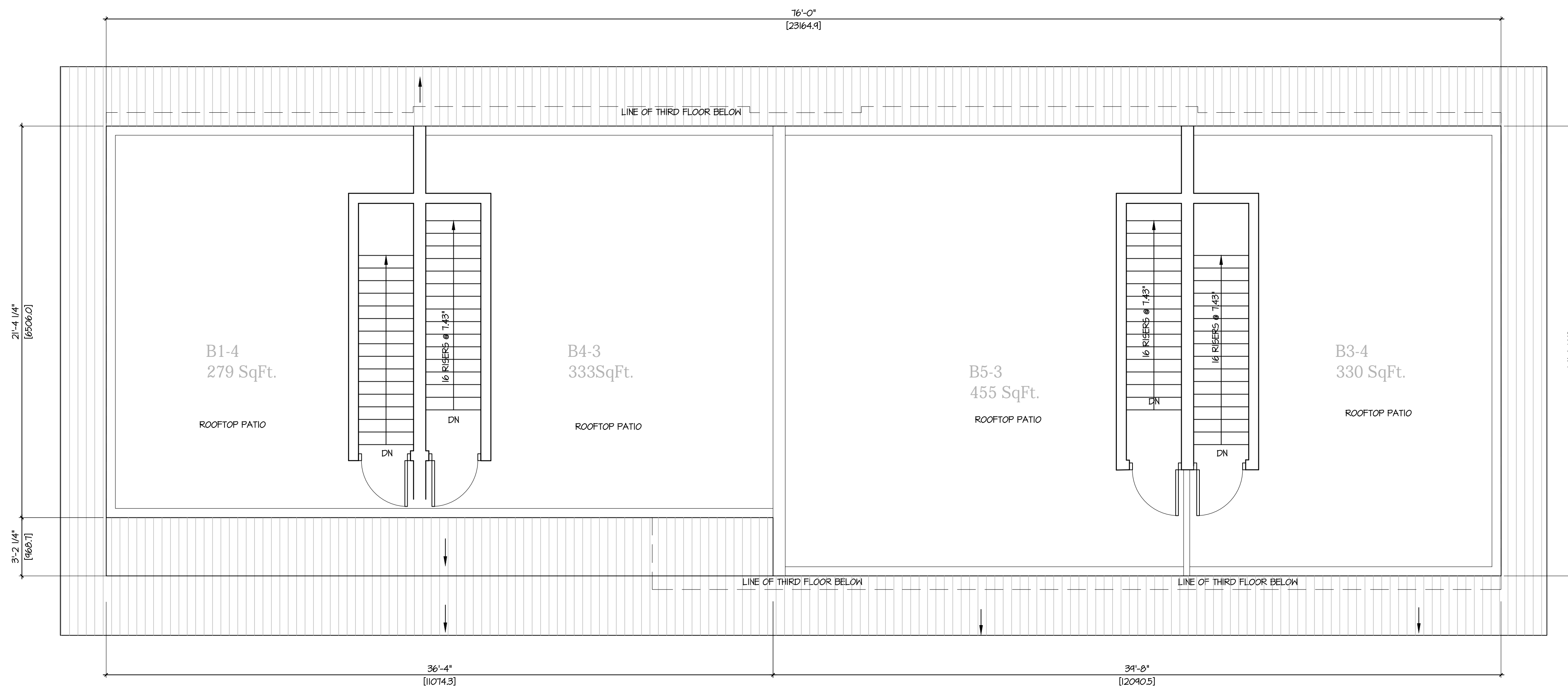
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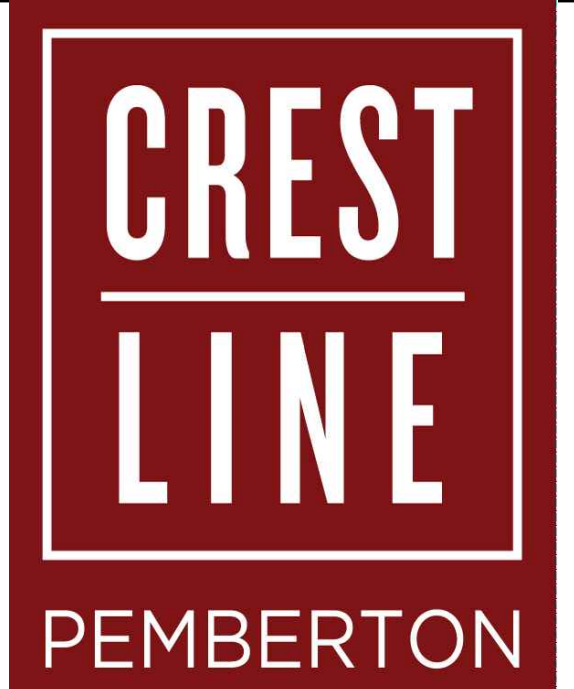




**BUILDING B - THIRD FLOOR**  
SCALE: 1/4" = 1'-0"



**BUILDING B - ROOF PLAN**  
SCALE: 1/4" = 1'-0"



REV	DATE	DESCRIPTION
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PROJECT  
**CRESTLINE**  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

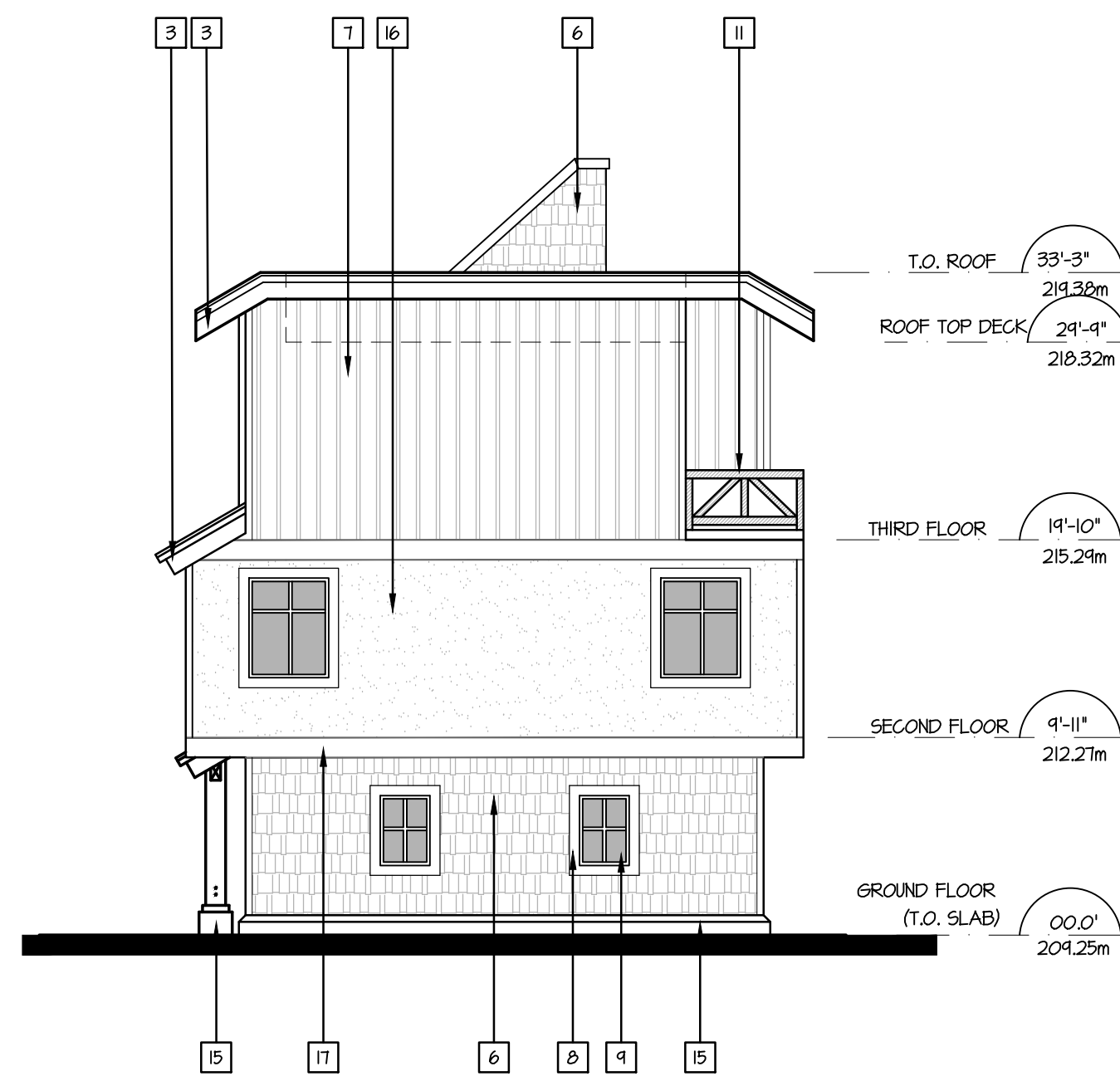
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BUILDING B 5 PLEX  
THIRD FLOOR & ROOF PLAN

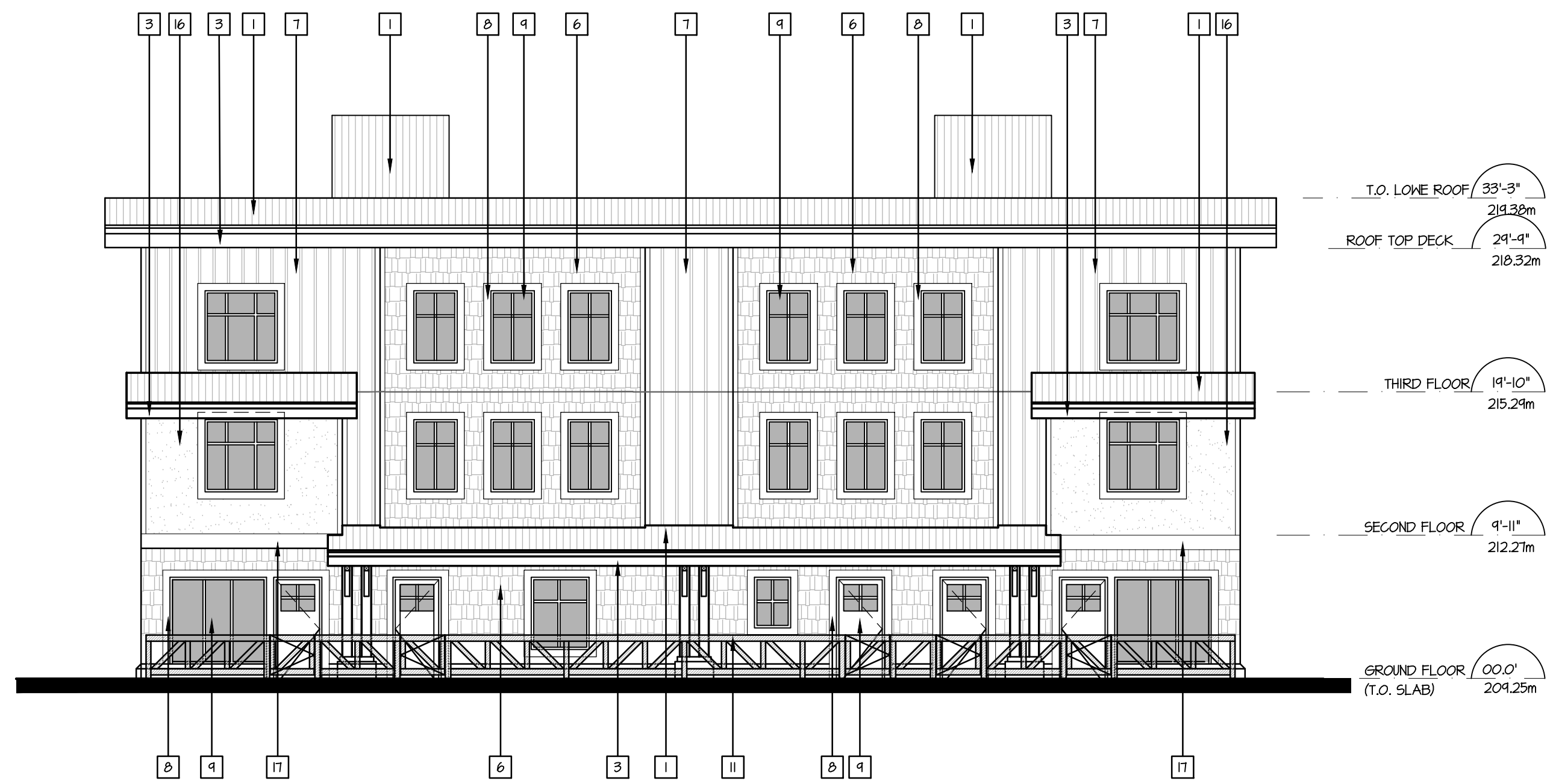
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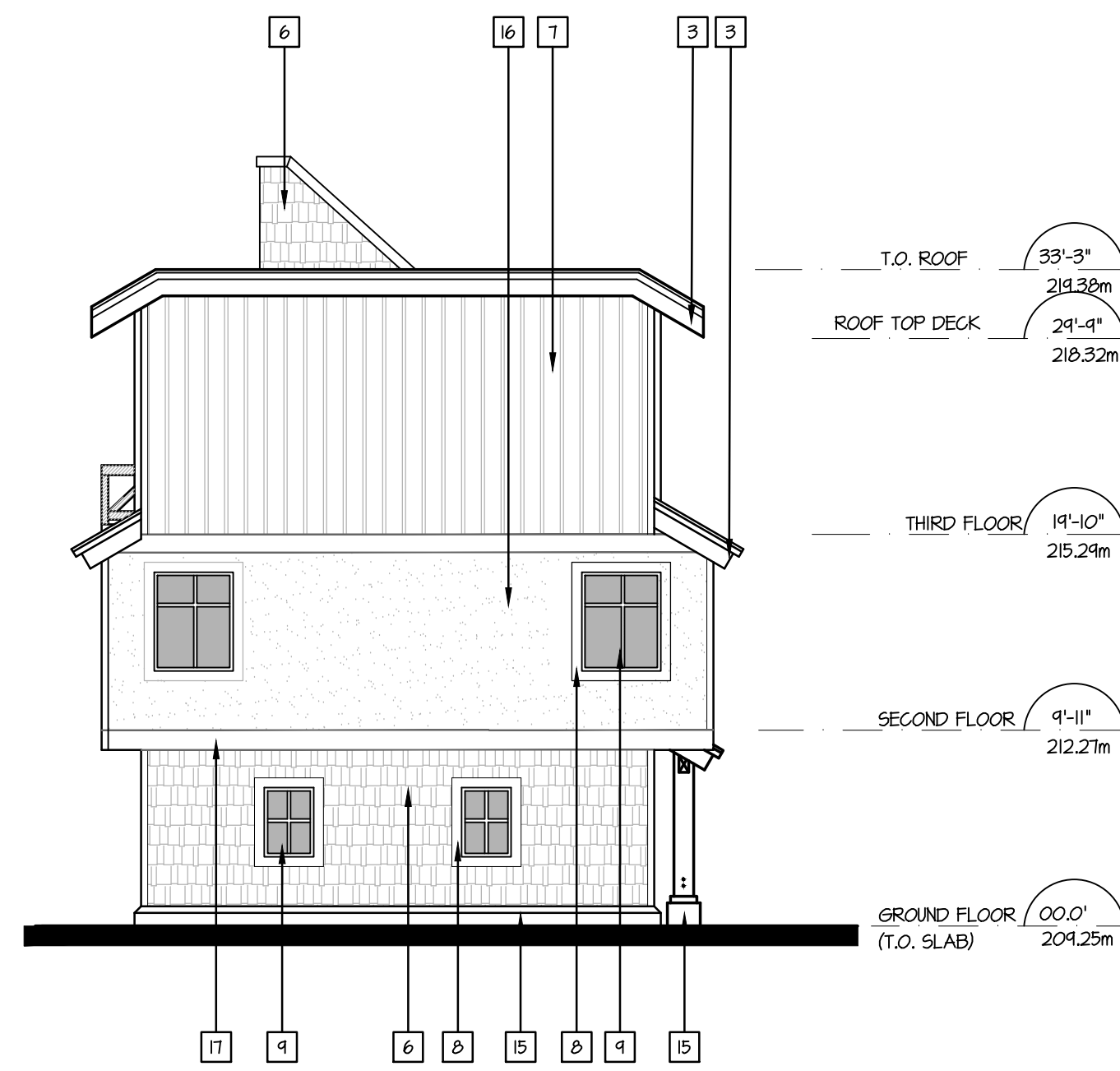
CRESTLINE 14-89



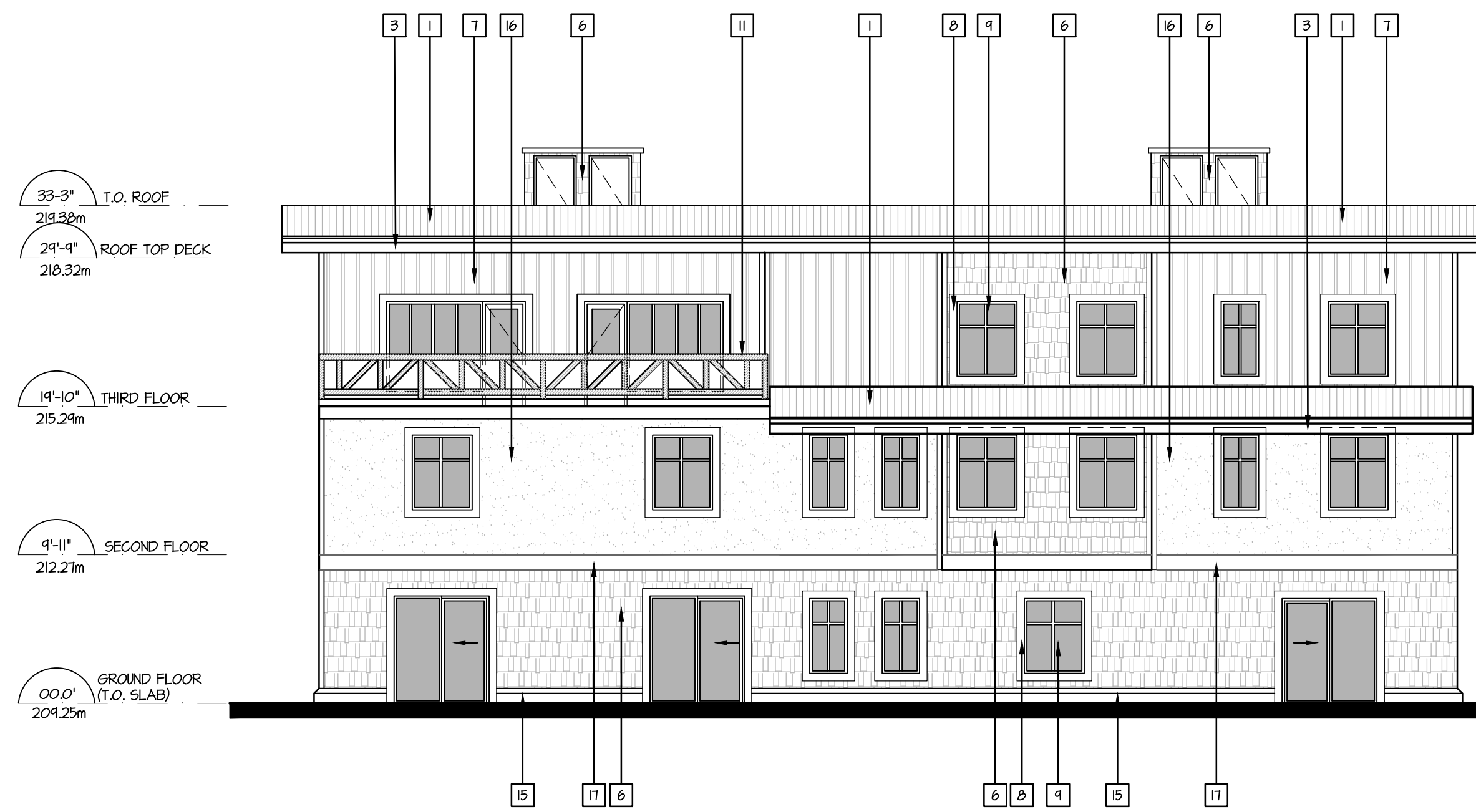
BUILDING B - WEST ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING B - NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING B - EAST ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING B - SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"

EXTERIOR FINISHES		* ALL MATERIAL FLASHING TO BE 24ga.
NO.	MATERIAL	COLOUR
1	STANDING SEAM ROOFING	CHARCOAL COLOUR
3	ROOF HARDIE TRIM #2	PAINTED - BM #2120-50 "SILVER SPRINGS"
5	DECORATIVE WOOD BRACES & COLUMNS	NATURAL - MAPLE STAIN
6	HARDI SHAKES	PAINTED LIGHT GREY TO MATCH BM #2120-50 "SILVER SPRINGS"
7	VERTICAL LAP SIDING 9.25" (EXPOSURE 8")	BM #2084-10 "BRICK RED"
8	HARDIE WINDOW/DOOR TRIM	PAINTED - BM #2120-50 "SILVER SPRINGS"
9	DOUBLE GLAZING WINDOWS & DOORS' FRAMES	INSULATED BLACK ANODIZED ALUMINUM
10	EXTERIOR INSULATED DOORS	INSULATED METAL
11	BALCONY ALUMINUM POSTS & RAILINGS	ALUMINUM
12	WOOD POSTS	NATURAL - MAPLE STAIN
13	EIFS COLUMN CAPS	NATURAL CONCRETE
14	STONE VENEER COLUMNS	CULTURED STONE - "GRAY COBBLEFIELD"
15	CONCRETE BASE	NATURAL CONCRETE
16	HARDI PANEL VERTICAL SIDING - STUCCO TEXTURE	COLOUR TO MATCH BM #HG-166 "KENDALL CHARCOAL"
17	HARDIE HORIZONTAL TRIM	PAINTED - BM #HG-166 "KENDALL CHARCOAL"
18	EXTERIOR LIGHT FIXTURES	FACTORY FINISH - BLACK

**CREST  
LINE**  
PEMBERTON

REV	DATE	DESCRIPTION
01	JAN 31 17	ISSUED FOR DP

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PROJECT: CRESTLINE  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

DRAWING

BUILDING B 5 PLEX  
ELEVATIONS

PROJECT NUMBER: 14-89 DRAWING NUMBER: A03

SCALE: 1/8" = 1'-0"

DATE: OCT 2014 REVISION: JAN 31 17

CRESTLINE 14-89









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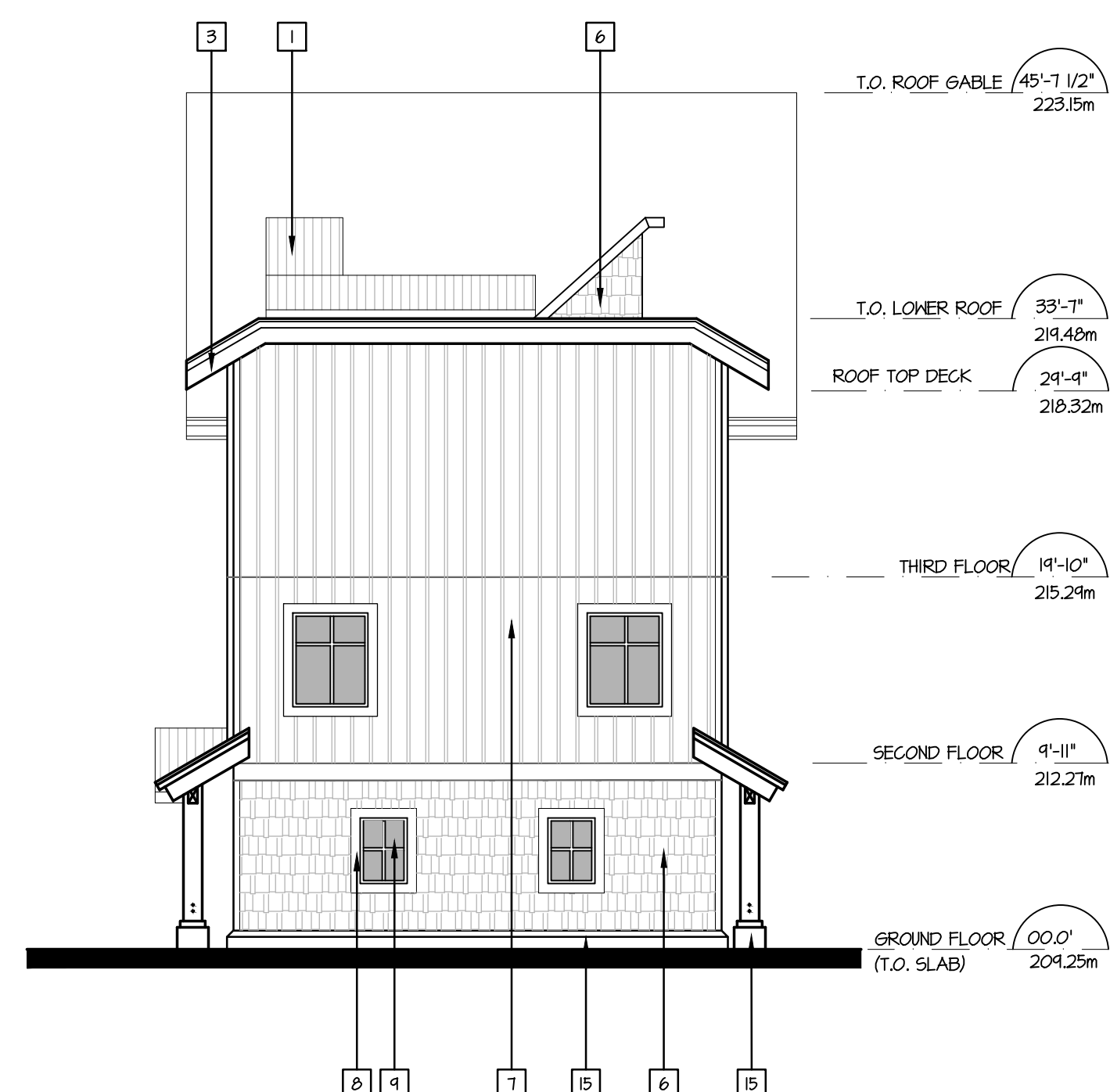
DRAWN BY AS APPROVED P.J.L.

PROJECT  
**CRESTLINE**  
 1422, 1426, 1430 PORTAGE ROAD  
 PEMBERTON, B.C.

DRAWING  
**BUILDING C & C MIRROR & PLEX ELEVATIONS**

PROJECT NUMBER DRAWING NUMBER  
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 SCALE  
 1/8" = 1'-0"  
 DATE  
 OCT 2014  
 REVISION  
 JAN 31 17

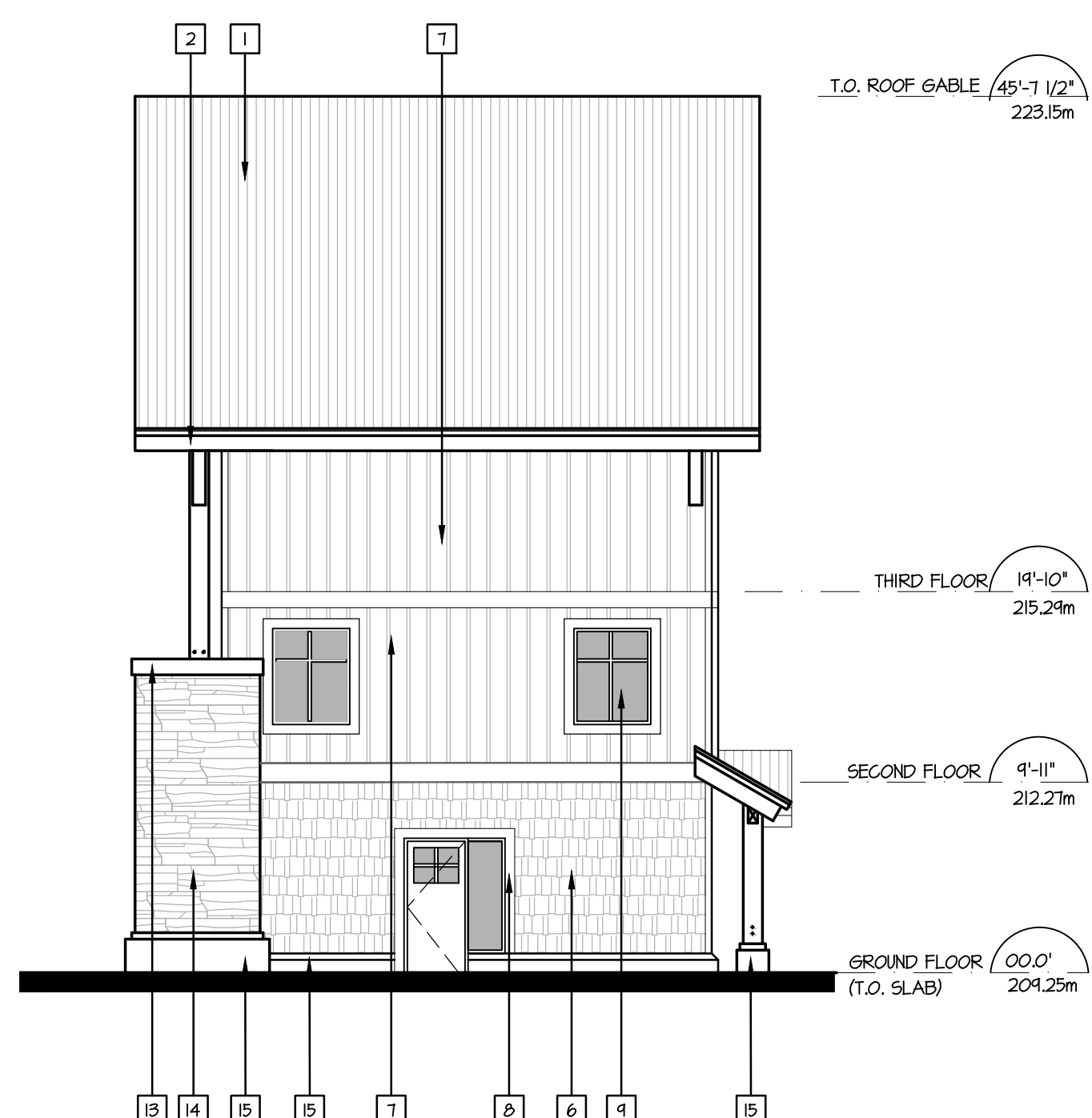
CRESTLINE 14-89



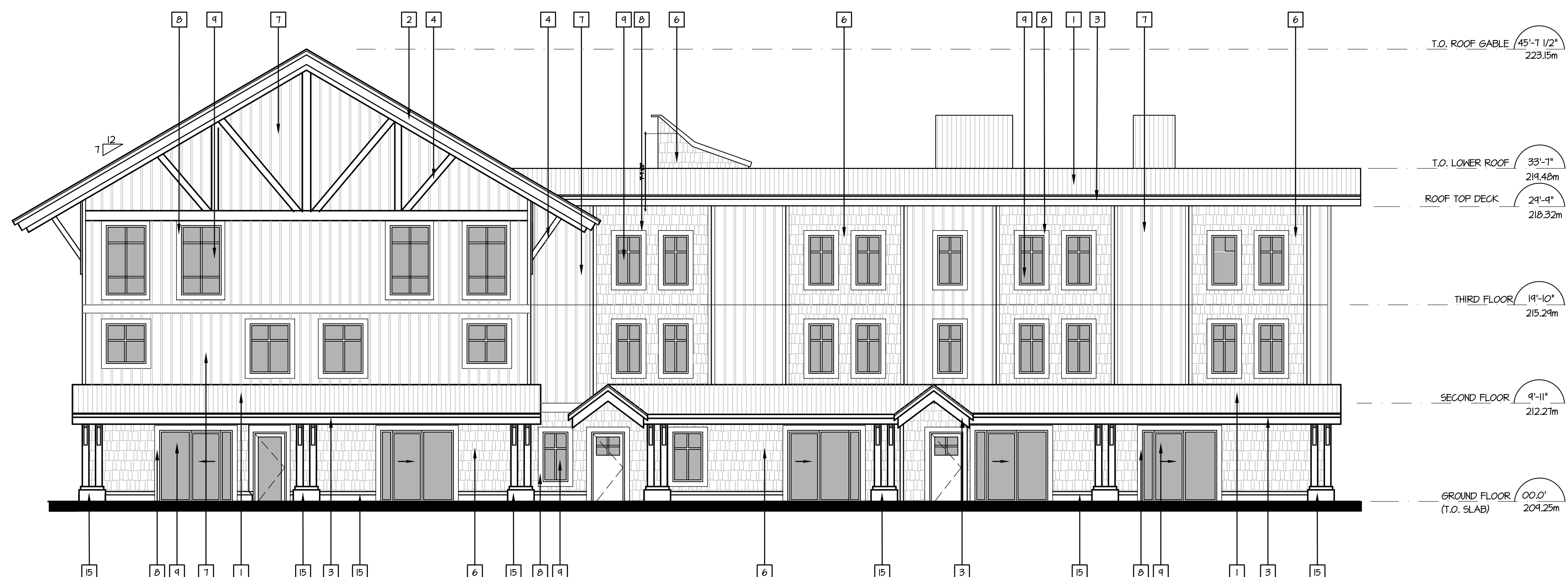
**BUILDING C & C (MIRROR) - EAST ELEVATION**  
 SCALE: 1/8" = 1'-0"



**BUILDING C & C (MIRROR) - NORTH ELEVATION**  
 SCALE: 1/8" = 1'-0"



**BUILDING C & C (MIRROR) - WEST ELEVATION**  
 SCALE: 1/8" = 1'-0"



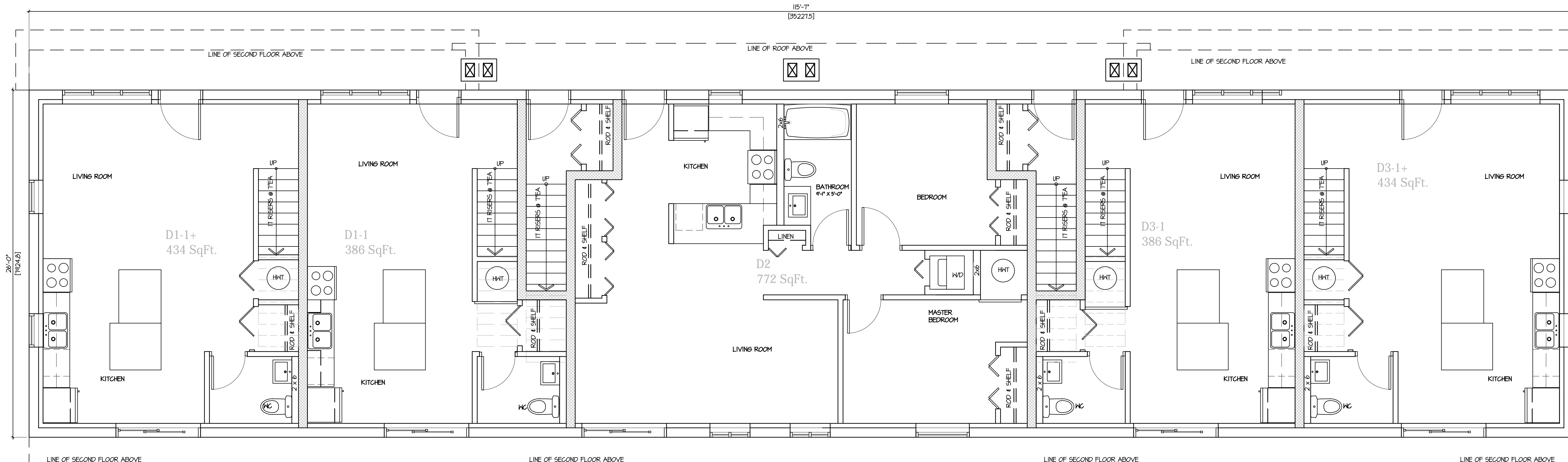
**BUILDING C & C (MIRROR) - SOUTH ELEVATION**  
 SCALE: 1/8" = 1'-0"

EXTERIOR FINISHES		* ALL MATERIAL FLASHING TO BE 24ga.
NO.	MATERIAL	COLOR
1	STANDING SEAM ROOFING	CHARCOAL COLOUR
3	ROOF HARDIE TRIM #2	PAINTED - EM #2120-50 'SILVER SPRINGS'
5	DECORATIVE WOOD BRACES & COLUMNING	NATURAL - MAPLE STAIN
6	HARDI SHAKES	PAINTED LIGHT GREY TO MATCH EM #2120-50 'SILVER SPRINGS'
7	VERTICAL LAP SIDING 4.25" (EXPOSURE 8')	EM #2084-10 'BRICK RED'
8	HARDIE WINDOW/DOOR TRIM	PAINTED - EM #2120-50 'SILVER SPRINGS'
9	DOUBLE GLAZING WINDOWS & DOORS' FRAMES	INSULATED BLACK ANODIZED ALUMINUM
10	EXTERIOR INSULATED DOORS	INSULATED METAL
11	BALCONY ALUMINUM POSTS & RAILINGS	ALUMINUM
12	WOOD POSTS	NATURAL - MAPLE STAIN
13	EIPS COLUMN CAPS	NATURAL CONCRETE
14	STONE VENEER COLUMNING	CULTURED STONE - 'GRAY COBBLEFIELD'
15	CONCRETE BASE	NATURAL CONCRETE
16	HARDI PANEL VERTICAL SIDING - STUCCO TEXTURE	COLOUR TO MATCH EM #14-166 'KENDALL CHARCOAL'
17	HARDIE HORIZONTAL TRIM	PAINTED - EM #14-166 'KENDALL CHARCOAL'
19	EXTERIOR LIGHT FIXTURES	FACTORY FINISH - BLACK

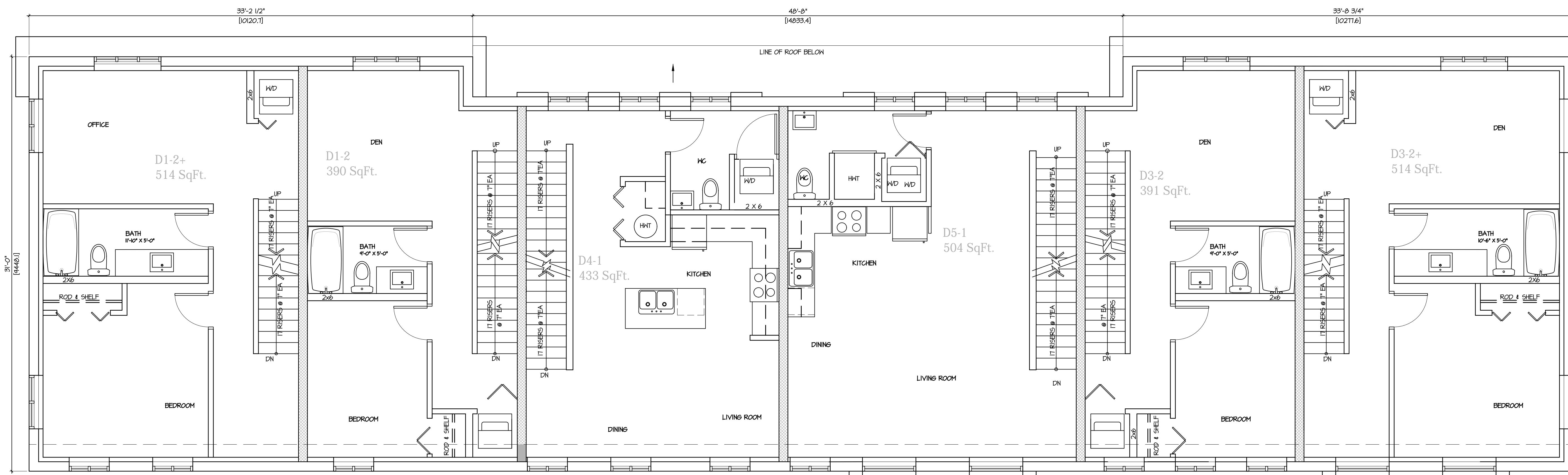




REV	DATE	DESCRIPTION
01	01/31/17	ISSUED FOR DP



**BUILDING D - GROUND FLOOR**  
SCALE: 1/4" = 1'-0"



**BUILDING D - SECOND FLOOR**  
SCALE: 1/4" = 1'-0"

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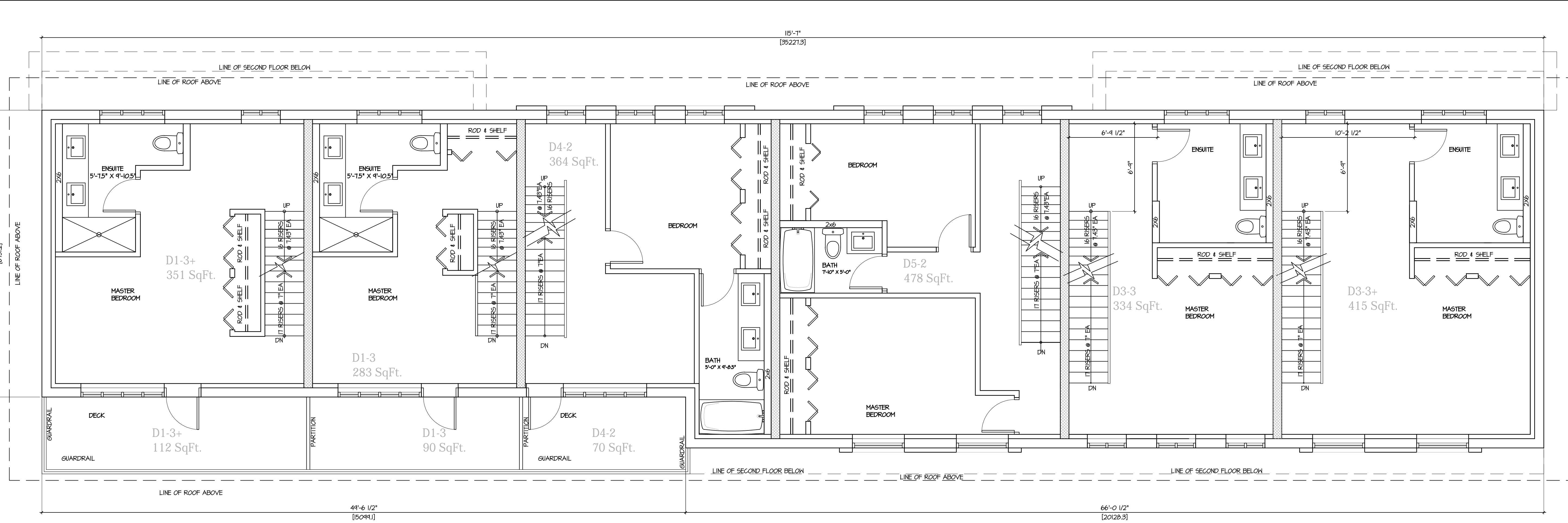
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APPROVED BY: P.J.L. PROJECT: P.J.L.

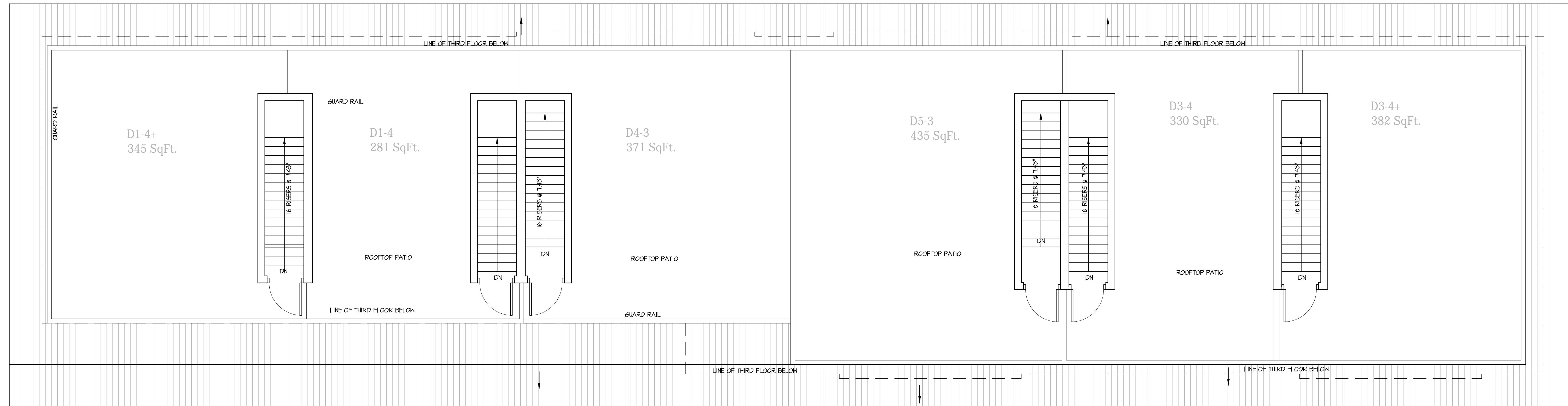
**CRESTLINE**  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

DRAWING: BUILDING D T PLEX  
GROUND & SECOND FLOORS  
PROJECT NUMBER: 14-89 DRAWING NUMBER: A01  
SCALE: 1/4" = 1'-0"  
DATE: OCT 2014 REVISION: JAN 31 17

CRESTLINE 14-89



BUILDING D - THIRD FLOOR  
SCALE: 1/4" = 1'-0"



BUILDING D - ROOF PLAN  
SCALE: 1/4" = 1'-0"

REV	DATE	DESCRIPTION
01	01/31/17	ISSUED FOR DP

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PROJECT: CRESTLINE

1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

DRAWING

BUILDING D T PLEX  
THIRD FLOOR & ROOF PLAN

PROJECT NUMBER: 14-89 DRAWING NUMBER: A08

SCALE: 1/4" = 1'-0"

DATE: OCT 2014 REVISION: JAN 31 17



01	01/31/17	ISSUED FOR DP
REV	DATE	DESCRIPTION

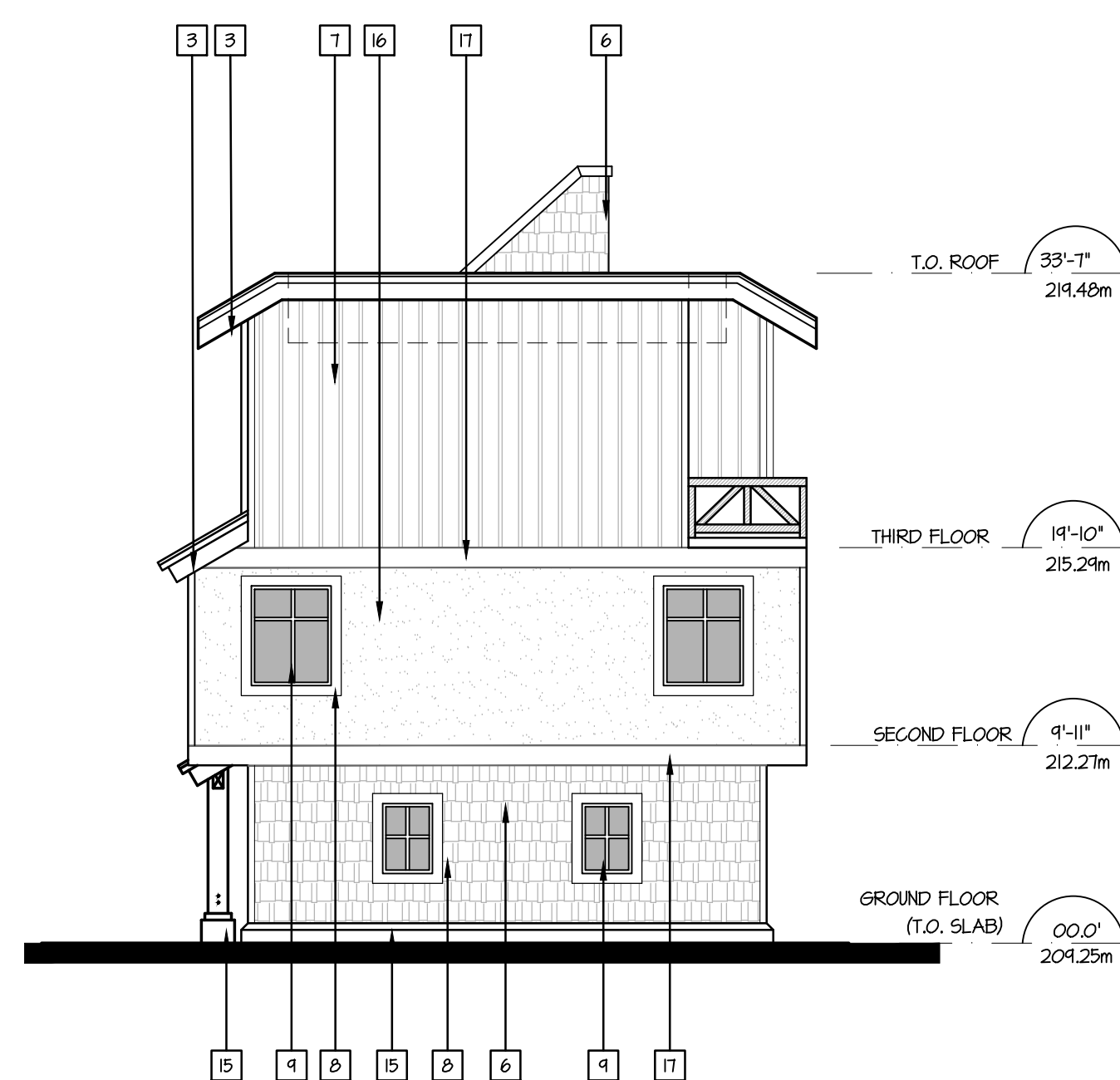
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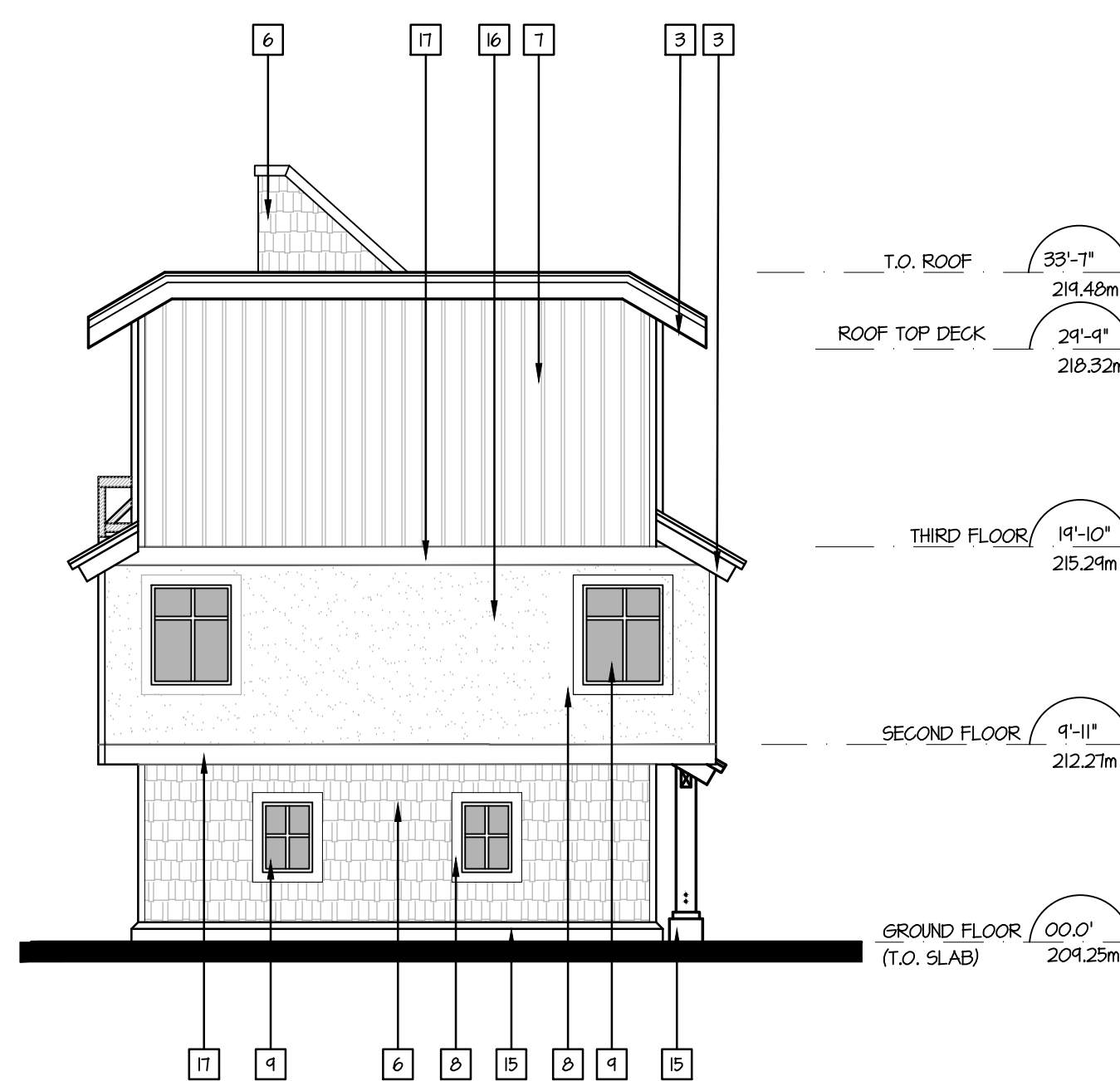
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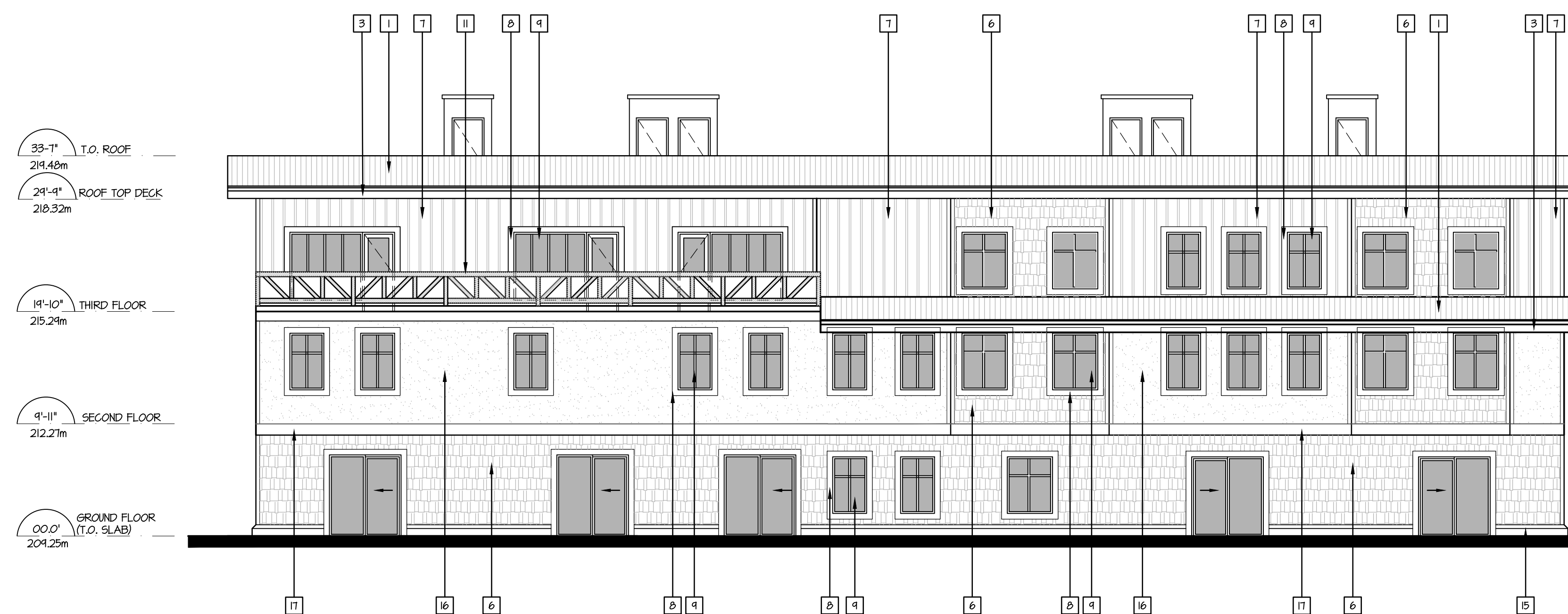
BUILDING D - WEST ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING D - NORTH ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING D - EAST ELEVATION  
SCALE: 1/8" = 1'-0"



BUILDING D - SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"

EXTERIOR FINISHES		* ALL MATERIAL FLASHING TO BE 24ga.
NO.	MATERIAL	COLOR
1	STANDING SEAM ROOFING	CHARCOAL COLOUR
3	ROOF HARDIE TRIM #2	PAINTED - BM #2120-50 'SILVER SPRINGS'
5	DECORATIVE WOOD BRACES & COLUMNS	NATURAL - MAPLE STAIN
6	HARDI SHAKES	PAINTED LIGHT GREY TO MATCH BM #2120-50 'SILVER SPRINGS'
7	VERTICAL LAP SIDING 4.25" (EXPOSURE 8")	BM #2084-10 'BRICK RED'
8	HARDIE WINDOW/DOOR TRIM	PAINTED - BM #2120-50 'SILVER SPRINGS'
9	DOUBLE GLAZING WINDOWS & DOORS' FRAMES	INSULATED BLACK ANODIZED ALUMINUM
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11	BALCONY ALUMINUM POSTS & RAILINGS	ALUMINUM
12	WOOD POSTS	NATURAL - MAPLE STAIN
13	EIPS COLUMN CAPS	NATURAL CONCRETE
14	STONE VENEER COLUMNS	CULTURED STONE - 'GRAY COBBLEFIELD'
15	CONCRETE BASE	NATURAL CONCRETE
16	HARDI PANEL VERTICAL SIDING - STUCCO TEXTURE	COLOR TO MATCH BM #14-166 'KENDALL CHARCOAL'
17	HARDIE HORIZONTAL TRIM	PAINTED - BM #14-166 'KENDALL CHARCOAL'
14	EXTERIOR LIGHT FIXTURES	FACTORY FINISH - BLACK

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DRAWN BY AS APPROVED P.J.L.

PROJECT  
**CRESTLINE**  
1422, 1426, 1430 PORTAGE ROAD  
PEMBERTON, B.C.

DRAWING

BUILDING D 7 PLEX  
ELEVATIONS

PROJECT NUMBER 14-89 DRAWING NUMBER A09

SCALE 1/8" = 1'-0"  
DATE OCT 2014 REVISION JAN 31 17

CRESTLINE 14-89

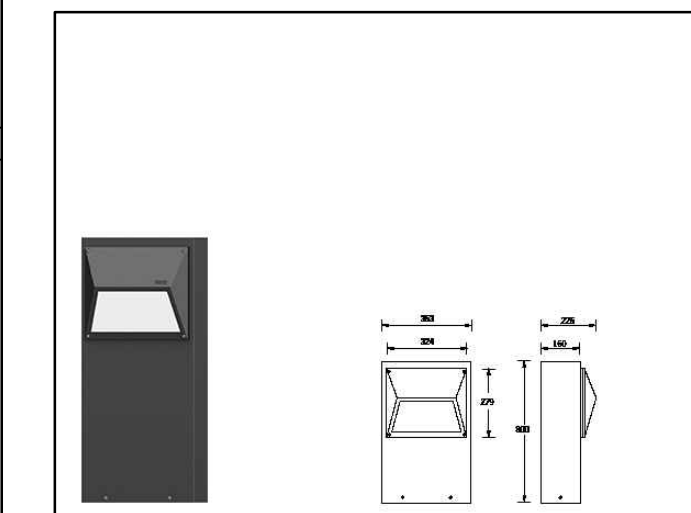
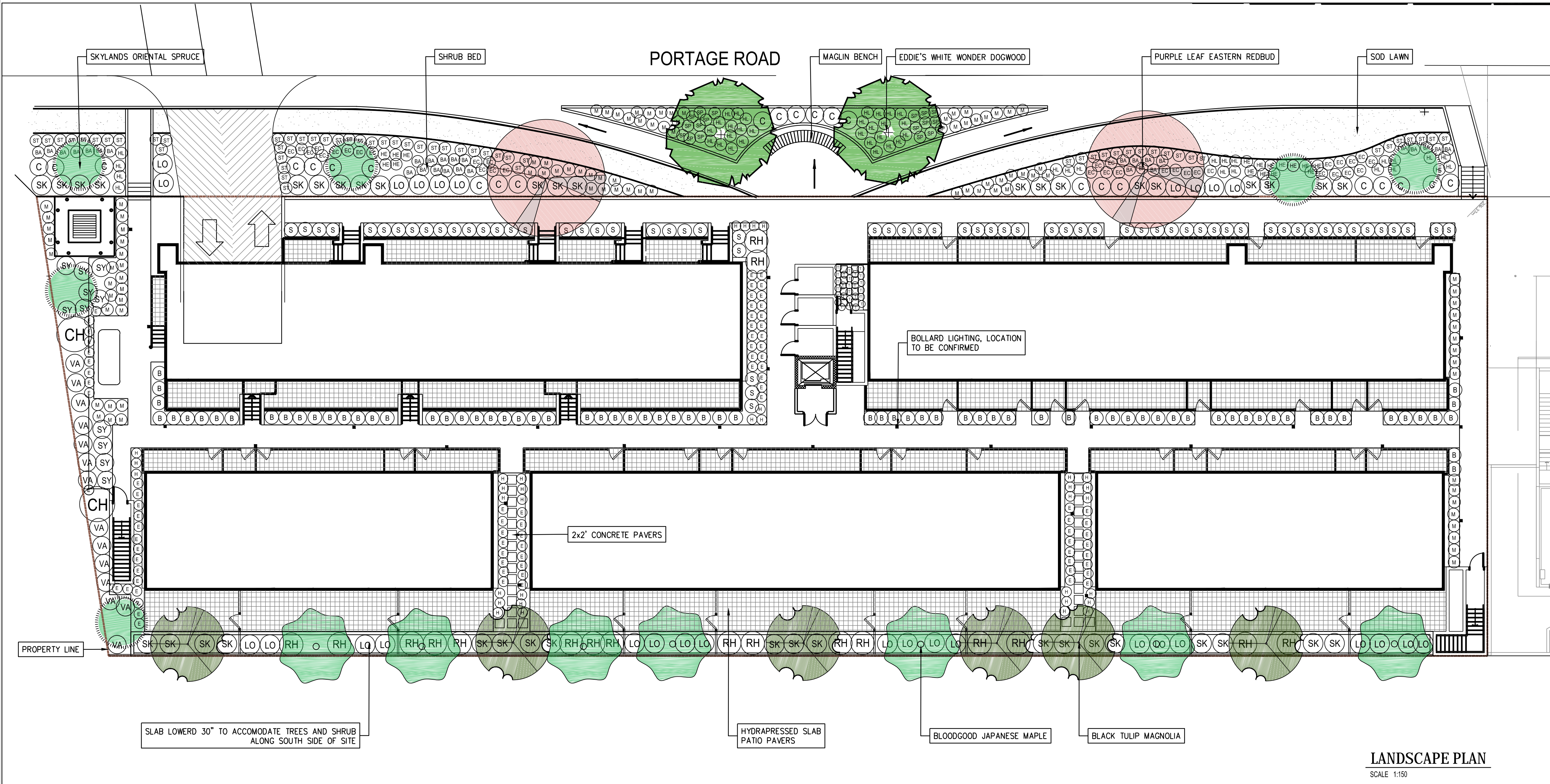


PLANT LIST

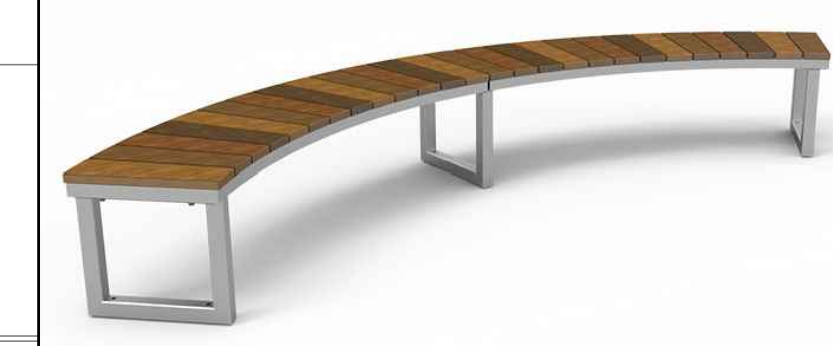
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	REMARKS
<b>TREE</b>						
7		ACER PALMATUM 'BLOODGOOD'	BLOODGOOD JAPANESE MAPLE	6CM CAL.	SEE PLAN	B-B WIRE BASKET
2		CERCIS CANADENSIS 'FOREST PANSY'	PURPLE LEAF EASTERN REDBUD	6CM CAL.	SEE PLAN	B-B WIRE BASKET
2		CORNUS 'EDDIE'S WHITE WONDER'	EDDIE'S WHITE WONDER DOGWOOD	6CM CAL.	SEE PLAN	B-B WIRE BASKET
6		MAGNOLIA 'BLACK TULIP'	BLACK TULIP MAGNOLIA	2.5m HT.	SEE PLAN	B-B WIRE BASKET
6		PICEA ORIENTALIS 'SKYLANDS'	SKYLANDS ORIENTAL SPRUCE	2.5m HT.	SEE PLAN	B-B WIRE BASKET
<b>SHRUBS</b>						
75		BUXUS SEMPERVIRENS	BOXWOOD	#3 POT	700mm	CONTAINER
24		CORNUS SERICEA	REDOSIER DOGWOOD	#3 POT	700mm	CONTAINER
2		CHAENOMELES JAPONICA 'ORANGE DELIGHT'	ORANGE FLOWERING QUINCE	#5 POT	SEE PLAN	CONTAINER
29		LONICERA NITIDA BAGGESEN'S GOLD	HONEYSUCKLE BAGGESEN'S GOLD	#3 POT	700mm	CONTAINER
18		RHODODENDRON CATAWBIENSE BOURSAILT	RHODODENDRON, PURPLE	#3 POT	700mm	CONTAINER
65		SARCOCOCOA RUSCIFOLIA	FRAGRANT SWEET BOX	#3 POT	700mm	CONTAINER
40		SKIMMIA KALEICOSCOPE	JAPANESE SKIMMIA	#3 POT	700mm	CONTAINER
18		SPIRAEA BUMALDA GOLDFLAME	GOLDFLAME SPRAEA	#2 POT	700mm	CONTAINER
11		SYMPHORICARPOS ALBUS	SNOWBERRY	#3 POT	700mm	CONTAINER
14		VACCINIUM OVATUM 'SCARLET OVATION'	EVERGREEN HUCKLEBERRY	#3 POT	700mm	CONTAINER
<b>GROUND COVER</b>						
83		EPIMEDIUM PERRALCHICUM FROHNLEITEN	BARRENWORT	4" POT	700mm	CONTAINER
94		MAHONIA NERVOSA	DWARF OREGON GRAPE	#2 POT	700mm	CONTAINER
<b>GRASS</b>						
48		HELICOTRICHON SEMPERVIRENS	BLUE OAT GRASS	#2POT	800mm	CONTAINER
56		STIPA TENUISSIMA	MEXICAN FEATHER GRASS	#2POT	800mm	CONTAINER
<b>PERENNIAL</b>						
30		BAPTISIA 'PURPLE SMOKE'	WILD INDIGO	#1POT	800mm	CONTAINER
33		ECHINACEA MAMA MIA	MAMA MIA CONEFLOWER	#2POT	800mm	CONTAINER
34		HOSTA JUNE FEVER	JUNE FEVER PLANTAIN LILY	#2POT	800mm	CONTAINER
16		HEMEROCALLIS INWOOD	INWOOD DAYLILY	#1POT	800mm	CONTAINER
26		THYMUS SERPYLLUM 'PINK CHINTZ'	PINK CHINTZ THYME	4" POT	450mm	CONTAINER



PLANT IMAGES



ECO BOLLARD  
N.T.S.  
REFER TO LIGMAN LIGHTING



MAGLIN BENCH OGM1900-00067  
N.T.S.  
REFER TO MAGLIN SITE FURNITURE

No.	DESCRIPTION	DATE	BY
2	ISSUED FOR DP	25/01/2017	RH
1	ISSUED FOR REVIEW	03/11/2016	RH

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contractor's responsibility to ensure that they are in  
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DESIGN CREW SEAL  
surveyed by  
drawn by RH  
designed by RH  
checked by BA

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LANDSCAPE ARCHITECTS  
SPORTS FACILITY DESIGNERS

DRAWING TITLE  
**LANDSCAPE  
PLAN**

AS SHOWN

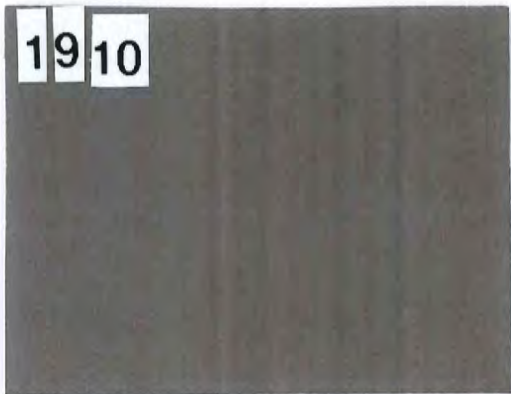
project no. 16-0583  
date November 3, 2016  
scale AS SHOWN

ISSUED FOR REVIEW

LANDSCAPE PLAN  
SCALE 1:150



EXTERIOR FINISHES		* ALL MATERIAL FLASHING TO BE 24ga.
NO.	MATERIAL	COLOUR
1	STANDING SEAM ROOFING	CHARCOAL COLOUR
2	ROOF HARDIE TRIM #2	PAINTED - BM #2120-50 "SILVER SPRINGS"
3	DECORATIVE WOOD BRACES & COLUMNS	NATURAL - MAPLE STAIN
4	HARDI SHAKES	PAINTED LIGHT GREY TO MATCH BM #2120-50 "SILVER SPRINGS"
5	VERTICAL LAP SIDING #25" (EXPOSURE 8")	BM #2084-0 "BRICK RED"
6	HARDIE WINDOW/DOOR TRIM	PAINTED - BM #2120-50 "SILVER SPRINGS"
7	WOOD POSTS	NATURAL - MAPLE STAIN
8	STONE VENEER COLUMNS	CULTURED STONE - "GRAY COBBLEFIELD"
9	HARDI PANEL VERTICAL SIDING - STUCCO TEXTURE	COLOUR TO MATCH BM #4C-166 KENDALL CHARCOAL
10	HARDIE HORIZONTAL TRIM	PAINTED - BM #4C-166 KENDALL CHARCOAL





# 7.0 Development Permit Guidelines

## 7.4 Development Permit Areas for the Form and Character of Development (Development Permit Areas 4, 5 & 6)

In accordance with Section 919.1 and 920 of the **Local Government Act**, Development Permits are required for areas which are hereby established and designated on Map C as DPA#4 (Downtown Revitalization), DPA #5 (Intensive Residential), DPA #6 Multi-family and/or Commercial Development, and DPA#7 (Gateway Commercial Development). This plan establishes both general form and character development permit objectives and guidelines that are applicable to each of DP#4, DP#5, DP#6 and DP#7 and then objectives and guidelines specific to each such development permit area.

### 7.4.1 General Form & Character Requirements

The Development Permit includes requirements respecting the form and character of the development.

#### 7.4.1.1 General Form & Character Objectives

- To provide a unifying and functional framework for quality and effectively integrated multi-family, mixed use (multi-family/commercial) and commercial development; and
- Showcase the area's natural features, heritage landmarks, open spaces and parks.

#### 7.4.1.2 General Form & Character Guidelines

The following general guidelines apply to DP#4 (Downtown Revitalization), DP#5 (Intensive Residential), DPA#6 (Multi-family and/or Commercial Development) and DP#7 (Gateway Development) and have incorporated specific directions related to site design, building form, landscaping, snow management, vehicle circulation, parking and servicing:

- a) Siting** - Development should recognize and complement the site's existing conditions, topography, natural vegetation, hydrology, solar exposure, site circulation and view corridors.
- Design for solar exposure to public and private spaces (summer shade and winter sun) and define and enhance the street edge in the placement and design of buildings and open spaces.
  - Provide a strong visual and physical relationships to pedestrian walkways and public spaces and provide opportunities for natural surveillance (eyes on the streets and open spaces).
  - Achieve privacy for residential units through inset balconies, decks and patios, and screening.
  - Provide barrier free access.



### What is a Defensible Zone?

A defensible zone is a space to protect buildings from approaching wild fire and to reduce the potential for a building fire to spread to the adjacent forest and shall be provided by the builder and maintained by the owner. Defensible zones shall ensure that:

- Annual grasses within 10 meters of buildings should be mowed to 10cm or less.
- Ground litter and downed trees should be removed annually.
- Any over storey trees retained within this zone should be away from the immediate area of the building and should be thinned and pruned to prevent fire from being carried towards the building.
- Remove the live and dead branches to a minimum of 2.5m (8 feet) from the ground.
- Tree cover within this zone should be restricted to low flammable deciduous species.
- Individual trees and shrubs may be kept if the vegetation does not readily transmit fire to the building.

Source: *Village of Pemberton Building Bylaw*



# 7.0 Development Permit Guidelines

**b) Building Form** - Buildings are to be consistent with Pemberton's small town character and reflect its rural traditions of strong, simple and functional building forms. It is not the intent that the Village adopt a specific architectural style or theme.

- Provide a cohesive design program for the development (i.e. structural, mechanical, lighting and landscaping).
- Avoid blank walls which are visible from the street or parks which lack architectural detailing
- Reduce the mass and scale of buildings through design features such as variations in roof form, wall recesses/projections, texture/colour, vertical accents, windows, balconies dormers and facade detailing.
- Design buildings to positively address the public realm on street frontages and sidewalks.
- Encourage decks, balconies and porches to provide sunny, usable outdoor spaces.
- Create interest with the roof structure using architectural features such as chimneys, cupolas, towers and venting. Roof mounted equipment should be concealed from pedestrian viewpoints.
- Provide visual variety along streetscapes by varying individual unit designs.
- Face main entrances to the street, being clearly visible and directly accessible from the sidewalk. Entrances should reinforce proximity to grade level, particularly avoiding multi-storey features. Diminish the appearance of garage doors from public streets.
- Preference for side by side, up and down or staggered unit configuration to maximize the number of units facing the street.
- Create interest by varying use of the building's scale, modulation, materials and colour in the placement and detailing of elements such as bay windows, entrances, lighting, graphics and street furnishings.

**c) Construction Materials** - The building should be sufficiently durable to withstand Pemberton's varied climate while also exhibiting quality construction and a small town character:

- Use exterior materials that have been traditionally applied and/or are durable for the area including stone, wood, brick, and glass.
- Discourage the use of the following exterior building materials: vinyl siding, plywood, particle board and synthetic materials such as cultured stone. Stucco and tile for large areas should be discouraged.
- Use well designed window treatments of articulated wood, stone or metal details. Reflective or heavily tinted glass and snap-in muntin bars are not recommended.
- Apply exterior building colours that complement nature's spectrum; earth hues and the natural colours of foliage, grass, sky and woods: Brighter colours may be appropriate as accents such as doorways, window frames, signs, graphics, store fronts and/or displays.
- Ensure approval of all playground and park infrastructure by the Canadian Standards Association.
- Complement neighbouring roof lines/pitches. Brightly-coloured metal roofs are discouraged.
- Anchoring buildings with continuous durable finishing providing a sense of permanence and protection from street level impacts.
- Wood roofing (Class A) will not be permitted under any circumstances as per the Village's Building Bylaw.
- Use muted or natural roof colours and where appropriate

**d) Streetscape Improvements and Landscaping** - The development should provide amenities for residents and visitors, while also adding interest to the street and showcasing local businesses.

- Incorporate planting consistent with the Village's standards contained within the Plant List.
- Consider four season landscaping for both aesthetics and maintenance reasons.
- Appropriately design, protect and select durable landscaping as not to be harmed by snow clearing or other maintenance works (i.e. sweeping).
- Prioritize the retention of existing trees and vegetation.

- Accommodate grading on individual properties, minimizing cut and fills and discourage retaining walls.
- Incorporate **Low Impact Development Techniques** into site, service and landscape planning
- Provide irrigation for all landscaped and open areas
- Incorporate landscaped areas within parking lots to break up large paved areas. The landscaped areas can also be used in the winter for snow storage.

**e) Circulation and Parking** - A development's accommodation of internal and external vehicular circulation, parking and servicing is an important consideration in the functioning and accessibility of the project.

- Screen or camouflage from public view all exterior services including utility tanks, hydro transformers, gas installations, garbage and recycling containers, preferably with a durable fenced enclosure, landscaping or printed art or images.
- Ensure that garbage and recycling receptacles and dumpsters are provided and located within a lockable building (bear proof).
- Accommodate efficient snow removal, including designated snow storage and drainage areas for access roads, loading and parking areas.
- Abide by the Village Construction Requirements as not to unsafely or inconveniently disrupt adjacent business operations or pedestrian movements during construction.

**f) Snow Management** - Site and building design shall mitigate the challenges related to freezing temperatures and precipitation.

All developments shall manage snow through the site plan design and building form.

- Restrict snow from dumping or being dumped onto adjoining streets, sidewalks and right of ways.
- Mitigate freeze / thaw cycle impacts including snow shed, roof drip, icicles, ice dams, and water infiltration.
- Prevent roofs from shedding towards pedestrian walkways, points of entry and loading or parking areas.

**g) Lighting** - Provide a lighting plan for new development. Fixtures on public roads shall be in accordance with Village Lighting Standards, while lighting on private property should be down shielded, as to illuminate only the desired display, pedestrian corridor, sign or building feature. Flashing, blinking or coloured lighting except for festival lighting is not supported.

**h) Crime Prevention Through Environmental Design Principles**

Development shall comply with the following principles:

- Provide clear border definition of controlled space.
- Provide clearly marked transitional zones that indicate movement from public to semi-public to semi-private to private spaces.
- Locate vehicle and pedestrian access points, gathering areas and loitering areas to locations with natural surveillance in order to increase safety and perception of safety of users, and increase risks (deterrent) to offenders.
- Design and land use should relate to the context of on-site land uses and structures, immediate adjacencies, and the surrounding neighbourhood.
- Site landscaping should have clear sight lines, prevent concealment, direct users safely, be permeable and maintain relationships (eyes on street).
- Re-designate the use of space to provide natural barriers to conflicting activities
- Seek land use mix that promotes natural surveillance.
- Overcome distance and isolation through improved communication, break-up large impersonal space, enhance sight lines, provide a range of land uses, and offer legitimate activity generators.
- Avoid building designs, public amenities/services and street furniture that create excuses for loitering, nuisance and criminal behaviour.



#### **What is Low Impact Development?**

An ecologically friendly approach to site development and storm water management that aims to mitigate development impacts on land, water and air ([www.lowdevelopmentimpacts.org](http://www.lowdevelopmentimpacts.org))



# 7.0 Development Permit Guidelines

## 7.4.2 Development Permit No. 4 - Downtown Revitalization

The Village encourages enhancements and redevelopment of the downtown area to provide a more vibrant environment for businesses, residents and visitors.

### 7.4.2.1 Objectives

The Village of Pemberton has established DPA#4 - Downtown Revitalization, in an effort to fulfill the following:

- Enhance Pemberton's authentic identity by providing a framework for the character and form of buildings, landscaping, streetscapes and circulation.
- Create a strong sense of arrival to the Pemberton community through natural and built gateway elements.
- Accommodate and integrate infrastructure needs with parking and transit
- Showcase and enhance the surrounding natural features, heritage landmarks, open spaces and parks.

### 7.4.2.2 Guidelines

The Development Permit Guidelines for commercial areas for revitalization have incorporated specific directions related to: building form; streetscape improvements and landscaping; circulation and parking; and signage and lighting.

**a) Building Form** - Buildings need to create pedestrian interest and memorable buildings by:

- Reflect the scale of the downtown with varied yet harmonious façade elements, adding interest to the downtown's building form.
- Avoid interior malls.
- Provide a functional roof covering along pedestrian oriented frontages to provide protection from the weather.
- Setback covered walkways from the face of upper stories; or extend partially or fully into the public realm.
- Coordinate adjoining buildings to maintain visual continuity of eave lines, materials, soffits and fascias. Either avoid the exposure of party walls or consider them as an important design feature.

- Accommodate as part of the roof fascia and the column design: flat signs; signs on brackets; banners on brackets; and exterior lights (with the appropriate scale and detail).
- Extend rooflines into the public right of way for aesthetics and weather protection, yet ensure it does not in conflict with emergency vehicle or pedestrian access and is secured through an encroachment agreement.



**b) Streetscape improvements and landscaping** - These enhancements provide amenities for residents and visitors, adding interest to the street and showcasing local businesses.

- Include streetscape fixtures such as street lighting, benches, planters, garage/recycling receptacles, bike racks, and landscaping features. These installations shall be in accordance with Village Streetscape Specifications and be functional, attractive and durable.
- Provide sidewalk extensions ("bump outs") at the intersections of Birch and Frontier Streets, Aster and Frontier Streets, Prospect and Frontier Streets and Birch and Prospect Streets. These spaces should be activated, where appropriate, for landscaping, seating and public art (but not interfere with pedestrian or vehicle movement).
- Provide opportunities for sun and shade as well as protection from the elements such as wind, rain and snow.



- Encourage site enhancements such as water features, public art, flags, banners and signs, provided they are consistent with village requirements (i.e. sign bylaw).
- Permit the installation and maintenance of fixtures (signs, tables, chairs and planters) on public property subject to the encroachment agreement from the Village and/or the Ministry of Transportation.

**c) Circulation and Parking** - A developments accommodation of internal and external vehicular circulation, parking and servicing is an important consideration of the functioning and accessibility of the project.

- Recognize that the primary function of a laneway is to service the accessing businesses. If appropriate, lanes have a secondary opportunity to provide pedestrian linkages, if safety and security issues can be addressed.
- Access service bays, loading docks and garbage/recycling dumpsters from existing laneways and screen utilities from public view. Access should be achievable during all weather conditions.
- Provide on-site parking either accessed from the rear of the development (lane) or underground. Parking is not permitted within the front yard setback, however, the enhancement of on-street parking along the frontage of the building is encouraged.

**d) Signage** - The installation of signs is intended to provide for business identification and safe travel but also contributes to an active and attractive streetscape.

- Provide a comprehensive sign program to ensure that the signs are integrated into and complement the project and the downtown.
- Ensure that signs are in accordance with the Village's Sign Bylaw. Variances to the Bylaw may be considered by Council.



Drawings from Downtown Enhancement Workshops



# 7.0 Development Permit Guidelines

## 7.4.3 Development Permit No. 5 - Intensive Residential

### 7.4.3.1 Objectives

The Village of Pemberton has established DPA#5 - Intensive Residential to ensure that neighbourhoods embrace and accommodate a mix of residential densities to facilitate livable, cohesive and compatible neighbourhoods.

### 7.4.3.2 Guidelines

**a) Siting** - The site design should minimize direct visual intrusion to surrounding homes. Privacy can be achieved through inset balconies, decks and patios, screening and/or off-setting windows.

**b) Building Form** - The development should break up larger buildings in to smaller units or clusters while ensuring that intensive residential developments reflect the scale and character of the area.

**c) Open Spaces** - The development should be designed to accommodate the appropriate passive and/or recreational activities of the neighbourhood and:

- Provide private outdoor open space for all units (i.e. patio, porch, deck, balcony, yard, etc).
- Consider fences only if they positive contribute to the attractiveness of the neighbourhood and will be durable over time.

**d) Circulation and Parking** - Parking areas, garages and driveways should appear as a minor component of the site when viewed from the street.

## 7.4.4 Development Permit No. 6 - Multi-family and/or Commercial Development

### 7.4.4.1 Objectives

The Village of Pemberton has established DPA#6 - Multi-family and/or Commercial Development in an effort to fulfill the following:

- Create livable and attractive neighbourhoods.
- Provide visual and physical cohesiveness that reflect our small town character.

### 7.4.4.2 Guidelines

**a) Siting** - The site design should minimize direct visual intrusion to surrounding homes. Privacy can be achieved through inset balconies, decks and patios, screening and/or off-setting windows.

**b) Building Form** - The development should break up larger buildings in to smaller units or clusters while ensuring that intensive residential developments reflect the scale and character of the area. Also provide sufficient and secured exterior accessed storage areas for each unit.

**c) Open Spaces** - The development should be designed to accommodate the appropriate passive and/or recreational activities of the neighbourhood and:

- Provide private outdoor open space for all units (i.e. patio, porch, deck, balcony, yard, etc).
- Consider fences only if they positive contribute to the attractiveness of the neighbourhood and will be durable over time.

**d) Circulation and Parking** - The accommodation of internal and external vehicular circulation, parking and servicing is an important consideration in the functioning and accessibility of a development.

- Situate residential parking areas, garages and driveways as a minor component of the site when viewed from the street.
- Fully screen from public view all exterior services including utility installations, garbage and recycling containers, preferably with a durable fenced enclosure and landscaping.

**e) Streetscape Improvements and Landscaping** - The development should provide amenities for residents and the surrounding neighbourhood.

- Install landscaping along all property lines, except where there is an access point.
- Provide irrigation for all landscaped yards and open areas (excluding areas undisturbed in their natural state).
- Utilize landscaping to provide definition for pedestrian corridors and defining private or semi-private spaces.
- Accommodate grading on individual properties, minimizing cut and fills and discouraging retaining walls.
- Incorporate landscaped areas within parking lots to break up large paved areas. The landscaping can also be used in the winter for snow storage areas.



## 7.4.5 Development Permit No. 7 - Gateway Development

### 7.4.5.1 Objectives

The Village of Pemberton has established DPA#7 - Gateway Development, in an effort to fulfill the following:

- Create a strong sense of arrival to the Pemberton community through natural, landscaped and built gateway elements.
- Provide visual and physical cohesiveness that draws interest to the community.
- Present services and accommodations targeted to the travelling public that create an attractive community identity and character.

### 7.4.5.2 Guidelines

**a) Open Spaces** - Development should be designed to incorporate open space for the purposes of outdoor seating, socializing and passive recreation of residents or the travelling public. The open areas can also provide a vegetated buffer between the highway and other land uses as well as for sound attenuation.

**b) Siting** - The development should be visible yet attractive from the highway; outdoor storage should not be visible from neighbouring properties, Highway 99 or other public roads.

**c) Landscaping** - The development shall be landscaped as to provide an attractive entry from Highway 99 and other public roads, specifically:

- Incorporate landscaped areas within parking lots to break up large paved areas. The landscaping can also be used in the winter for snow storage areas.
- Provide a landscaped buffer between land uses
- Limit the use of fencing when not visible to public streets. Chain link fencing is not recommended.
- Provide irrigation for all landscaped yards and open areas (excluding areas undisturbed in their natural state).



**d) Circulation and Parking** - Vehicular circulation, parking and servicing is an important consideration in the functioning and accessibility of a development, whereby:

- Access to service bays, loading docks and garbage/recycling dumpsters should not be directly visible from public view. Access should be achievable during all weather conditions.
- Enhance parking areas with landscaping in the front yard setback
- Provide short term parking and unloading areas for accommodation uses.
- Screen parking areas and car staging (drive-throughs) to public street with landscaping and buildings.